

TOWN OF WELLESLEY



MASSACHUSETTS

SELECT BOARD

TOWN HALL • 525 WASHINGTON STREET • WELLESLEY, MA 02482-5992

MARJORIE R. FREIMAN, CHAIR
THOMAS H. ULFELDER, Vice Chair
COLETTE E. AUFRANC, Secretary
ELIZABETH SULLIVAN WOODS
KENNETH C. LARGESS III

FACSIMILE: (781) 239-1043
TELEPHONE: (781) 431-1019 x2201
WWW.WELLESLEYMA.GOV
MEGHAN C. JOP
EXECUTIVE DIRECTOR OF GENERAL GOVERNMENT SERVICES

SELECT BOARD MEETING

AGENDA

HYBRID MEETING

6:30 pm Tuesday, January 27, 2026

Juliani Room, Town Hall

525 Washington Street

<https://www.wellesleymedia.org/live-streaming.html>

View on TV on Comcast 8/Verizon 40

1. 6:30 Call to Order – Open Session
2. 6:31 Citizen Speak
3. 6:35 Executive Director's Report
4. 6:40 Consent Agenda
 - Vote Common Victualler Modification of seating for Truly's at 35 Grove Street
5. 6:45 Discuss and Vote PSI-25-04 Traffic Recommendation for 592 Washington Street
6. 7:15 Discuss and Vote Winter Supplemental to Department of Public Works
7. 7:25 FY27 Budget Update
 - Discuss FY27 Operational and Capital Budgets
 - Vote Select Board Operational Budgets
 - Discuss Cash Capital Reductions and Carryforward Analysis
8. 7:45 Annual Town Meeting (ATM) Preparation
 - Article 6: Discuss and Vote Town Clerk Salary
 - Article 27: Discuss and Vote use of \$8,000 of TNC Funds (Uber/Lyft)
9. 8:10 Citizen Leadership Academy Update
10. 8:30 Administrative Matters
 - Discuss and Vote Minutes
11. 8:35 Chair's Report
12. 8:40 Executive Session M.G.L c. 30A §21A, exemption #3 to conduct strategy with respect to the Wellesley Firefighters, IAFF, Local 1795, Wellesley Free Library Staff Association and Wellesley Free Library Supervisor Association.

* 592 Washington Street – PSI Traffic Recommendation

Notice was issued for discussion of the PSI traffic recommendation at the 1/20/26 Meeting. Due to the need for additional information and clarification, the item was deferred to this meeting.

2026 Annual Town Meeting Budget Documents for Fiscal Year 2027 can be found at:

www.wellesleyma.gov/2026budgetbook

This page will be updated throughout the budget season

Next Select Board Meeting: February 10, 2026

[Please see the Select Board Public Comment Policy](#)

CITIZEN SPEAK PROTOCOL

Residents seeking to provide public comment on topics identified on the Board's agenda shall email sel@wellesleyma.gov prior to the meeting and you will be added to the list of speakers. Residents shall verbally identify themselves by name and address prior to commenting. All comments shall be addressed to or through the chair or acting chair of the Board. Each comment period shall not exceed 15 minutes and each speaker shall not exceed three minutes, unless otherwise determined by the Chair.



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EXECUTIVE DIRECTOR OF GENERAL GOVERNMENT SERVICES

The Select Board will be meeting on January 27, 2026, at 6:30 pm in the Juliani Room at Town Hall. This meeting will be a hybrid meeting with some participants joining via Zoom.

1. **6:30 Call to Order – Open Session**
2. **6:31 Citizen Speak**
3. **6:35 Executive Director's Update**
4. **6:40 Consent Agenda**

MOTION

MOVE to approve the Consent Agenda items.

- **Vote to Approve Increased Seating and Hours of Operation of Truly's located at 35 Grove Street.**

Truly's redesigned the space at its 35 Grove Street location to accommodate 35 seats for overflow seating from the main restaurant space located at 39 Grove Street. The overflow is intended to accommodate additional customers at peak times, as well as being used for private parties and other events. Included in the FNM packet is a rendering of the redesigned space. Truly's management confirmed with Support Services Manager Cay Meagher that the hours of operation will be identical to the hours at 39 Grove Street – 7:00AM-11:00AM, Sunday through Saturday. The redesigned space has been inspected and approved by the Health and Building Departments.

MOTION

MOVE to approve the increased seating request of Truly's at 35 Grove Street to 35 interior seats and to approve the hours of operation of 7:00AM-11:00PM, Sunday through Saturday, at the same location.

Truly's
39 Grove Street
Wellesley, MA 02492

January 19, 2026

Subject: Request for guest count increase for 35 Grove's CV

Dear Cathryn,

Please accept this formal request to increase the total seating capacity for the Common Victualler's License of Next Door at 35 Grove Street to 35 guests. I have attached multiple documents which list this number from previous communications.

The attachments include the Statement of Use and Contract Documents, Construction Drawings and Specifications Issued for Permit. I have also attached the Town Code Report and Project Summary, which lists the occupant code on page 4.

I would expect the timeline boundaries of use for Next Door to go from 7am – 11pm each day. If you have any additional questions, please do not hesitate to contact me. I appreciate your time and assistance with this adjustment.

Sincerely,

Jay Adrian

Director of Operations/General Manager

Statement of Use

Truly's

35 Grove Street

Wellesley, MA 02482

March 7, 2025

Cathryn Meagher

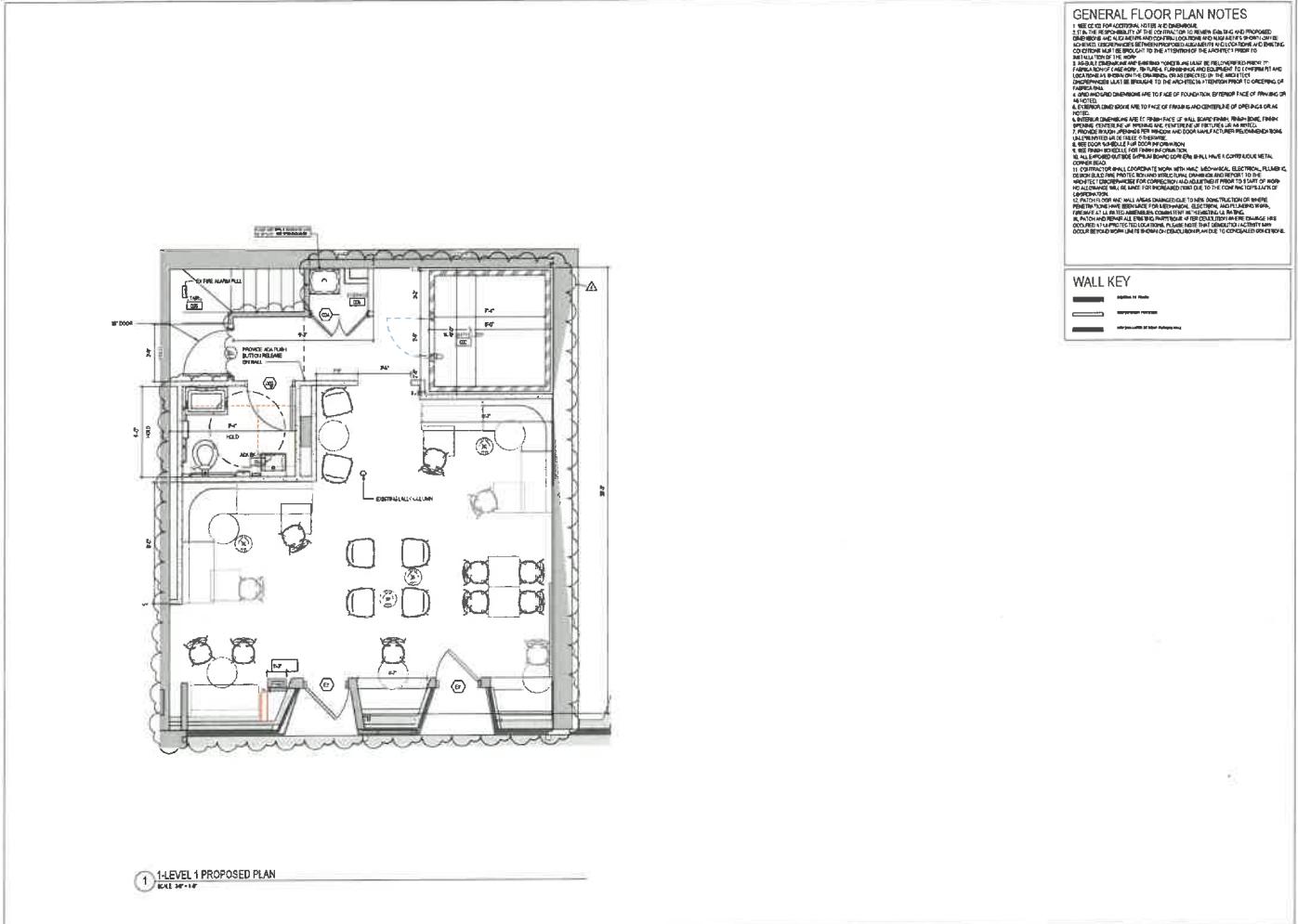
Town of Wellesley

525 Washington Street

Wellesley, MA 02482

This Statement of Use outlines the intended purpose, application, and scope of usage for the buildout of 35 Grove Street by Truly's.

The purpose of the 35 Grove Street build out is multi-faceted. The space per code allows for 35 total seats. Uses of the space are for ice cream storage, overflow seating for patrons of 39 Grove Street to sit and eat their ice cream during the busy hours, and for small events such as book clubs, birthday parties, and small meetings. No food will be prepared in the space. There will be an employee of Truly's that will monitor both inside and directly in front where there is existing seating.



Truly's

5. 6:45 Discuss and Vote PSI-25-04 Traffic Recommendation for 592 Washington Street

The Planning Board received a Project of Significant Impact (PSI) application for the renovation and expansion of 592 Washington Street in December 2025. The applicant's traffic study and other application materials are included in the FNM packets. The complete list of all materials submitted can be found here:

<https://wellesleyma.gov/1479/Planning-Board-Applications-Under-Review>

Tighe & Bond conducted the Traffic Peer Review on behalf of the Town and submitted its report to Meghan on January 14th. Meghan, Sheila, Corey, and Planning Department staff felt that there were a number of outstanding questions remaining in the peer review and requested additional information from Tighe & Bond. That information was received and staff now recommend that the Select Board review and approve the Peer Review and accompanying recommendation letter as drafted by Transportation and Mobility Manager Sheila Page (and reviewed by the full Traffic Committee).

MOTION

MOVE to approve the Traffic Peer Review prepared by Tighe and Bond as professionally conducted, and to issue the accompanying recommendation letter to the Planning Board.



**WELLESLEY PLANNING BOARD
APPLICATION FORM FOR REVIEW OF A
PROJECT OF SIGNIFICANT IMPACT**

DATE: 11/24/25

ADDRESS OF PROPERTY: 592 WASHINGTON ST, WELLESLEY, MA 02482 PRECINCT G

NAME OF OWNER OF RECORD: 592 WASHINGTON LLC

signature

EXISTING USE OF

LAND/BUILDINGS: OFFICE BUILDING

PRESENT ZONING: WELLESLEY SQUARE COMMERCIAL DISTRICT

PROPOSED USE OF

LAND/BUILDINGS: MULTI-FAMILY HOUSING (RESIDENTIAL)

FLOOR AREA OF BUILDING(S) NOW EXISTING ON THE SITE: _____ SQUARE FEET.

TOTAL FLOOR AREA OF BUILDING(S) PROPOSED ON THE SITE: _____ SQUARE FEET.

AGGREGATE TOTAL FLOOR AREA OF PROPOSED NEW CONSTRUCTION ONLY _____ SQUARE FEET.

(IF RESIDENTIAL) NUMBER OF DWELLING UNITS 19

AREA OF LOT OR DEVELOPMENT SITE 35,861 SQUARE FEET.

CONSULTANT(s) FOR IMPACT ANALYSIS METROWEST ENGINEERING - SITE/PLAN 508-626-0063 Phone _____

VANASSE & ASSOCIATES - TRAFFIC ENGINEER. (978) 474-8800

MCKAY ARCHITECTS 781-326-5400

This portion to be completed by Planning Department

APPLICATION FORM AND IMPACT ANALYSIS AS REQUIRED BY PLANNING BOARD RECEIVED BY _____

signature date

REVIEW COMPONENTS WAIVED BY PLANNING BOARD

_____ date of vote _____

SUBMISSION FEE RECEIVED \$ _____ date

IMPACT ANALYSIS TRANSMITTED TO REVIEW DEPARTMENTS _____ date

IMPACT ANALYSIS APPROVED BY:

Board of Selectmen _____ date

Board of Public Works _____ date

Fire Chief _____ date

Special Permit Approved by Planning Board _____ date.

WELLESLEY PLANNING BOARD

DEFINITIVE SUBMISSION - PROJECT PLAN SPECIFICATIONS

PROJECT OF SIGNIFICANT IMPACT

The Definitive Submission Project Plan shall be drawn to a scale of 1"=40' and shall show:

- a. Title and North arrow;
- b. Name of owner of record;
- c. Name of applicant (if different than owner);
- d. Names of all abutters as they appear on the most recent tax list;
- e. The general topography including an indication of open and wooded areas, permanent monuments, natural objects such as waterways, drainage courses, large boulders or ledge outcroppings, stone walls and the like;
- f. Proposed location of building(s) and structures, roads, drives, and parking areas, with the proposed rough layout of storm drains, water supply, sewage disposal system and necessary easements;
- g. The general relation of the proposed driveway(s), water, sewer and drainage systems and easements to adjoining properties and ways.

Should the plan be submitted on more than one sheet, all sheets shall be of the same size.

WELLESLEY PLANNING BOARD

Application Form

Planning Department
888 Worcester Street, Suite 160
Wellesley, MA 02482



Tel. (781) 431-1019 ext. 2232

Fax (781) 237-6495

Eric Arbeene, AICP, Interim Planning Director

Name of applicant: <u>592 WASHINGTON LLC</u>	Land owner(s): <u>592 WASHINGTON LLC</u>	Address or location of property: <u>592 WASHINGTON ST, WELLESLEY MA</u>
Mailing Address: <u>869 WORCESTER ST</u>	Mailing address: <u>869 WORCESTER ST</u>	Area in square feet: <u>35,861</u>
Phone: <u>(781) 235-2555</u>	Phone: <u>(781) 235-2555</u>	Town Plan number: <u>124</u>
Type of application: <ul style="list-style-type: none"> <input type="checkbox"/> Approval Not Required Plan (ANR) <input type="checkbox"/> Preliminary Subdivision Plan <input type="checkbox"/> Definitive Subdivision Plan <input type="checkbox"/> Plan to Upgrade Inadequate Way <input checked="" type="checkbox"/> Other <u>WELLESLEY SQ.</u> 	Materials and information required: <ul style="list-style-type: none"> For ANR submissions: <ul style="list-style-type: none"> <input type="checkbox"/> Original and six prints of all plan(s) For Subdivision Plan submissions: <ul style="list-style-type: none"> <input type="checkbox"/> Original and six prints of all plan(s) <input type="checkbox"/> Runoff calculations and other submission materials <input type="checkbox"/> Municipal systems analysis for subdivisions of 5 or more lots or 2 or more acres <input type="checkbox"/> Plans to Board of Health <input type="checkbox"/> Notice to Town Clerk 	FOR ALL APPLICATIONS <p>Name of land surveyor: <u>METROWEST ENGINEERING</u></p> <p>Mailing address of surveyor: <u>75 FRANKLIN ST, FRAMINGHAM MA</u></p> <p>Phone number of surveyor: <u>508-626-0063</u></p> <p>Zoning and area district: <u>WELLESLEY SQ COMMERCIAL DISTRICT</u></p> <p>Present use of property: <u>OFFICE BUILDING</u></p> <p>Proposed use of property: <u>MULTI-FAMILY HOUSING (RESIDENTIAL)</u></p> <p>FOR SUBDIVISION AND STREET UPGRADE PLANS</p> <p>Name of engineer:</p> <p>Number of lots:</p> <p>Length of street to be built:</p> <p>AREA IN SQUARE FEET OF</p> <p>Street right of way:</p> <p>Paved travel way:</p> <p>Total combined lot area:</p> <p>Estimated impervious cover including new roof surface:</p>
DO NOT WRITE IN THIS BOX		
To be completed by Planning Staff		
Date submitted: _____		
Action Required By: _____		
Action Taken: _____		
Date: _____		
File designation: _____		
I give permission for agents of the Planning Board to enter upon my land for purposes related to this application during regular business hours.	Signature of land owner: _____	Signature of applicant (if other than owner): _____

592 Washington LLC
869 Worcester St
Wellesley, MA 02482

November 24th, 2025

Wellesley Planning Board

Town of Wellesley
525 Washington Street
Wellesley, MA 02482

Re: Special Permit Request – 3-Story, 19-Unit Condominium Building

Dear Members of the Wellesley Planning Board:

On behalf of **592 Washington LLC**, I respectfully request approval of a **Special Permit** for a **three-story, 19-unit high-end condominium building** of approximately **50,000 square feet**, including **19 underground parking spaces**, at **592 Washington St, Wellesley, MA 02482**.

This project has been carefully designed to fit seamlessly within the surrounding neighborhood. The building's architecture, scale, and materials were selected to complement nearby homes, while underground parking eliminates the need for expansive surface lots and significantly reduces visual and traffic impact. The units will be high-quality, for-sale residences that meet the growing demand for well-designed, low-maintenance living options in Wellesley.

Throughout the planning process, we prioritized respectful integration with abutting properties. Landscaping, privacy buffers, and lighting controls have been incorporated to ensure a quiet, attractive, and well-screened setting. The development will also use modern, energy-efficient building systems and follow all state and local requirements for stormwater management and site safety.

We believe the proposal meets the Town's standards for Special Permit approval by preserving neighborhood character, ensuring safe circulation, and providing high-quality residential housing in a responsible manner. Our team is committed to collaborating with the Town and residents to deliver a project that enhances the surrounding area and reflects Wellesley's long-standing expectations for thoughtful design.

Thank you for your time and consideration. We welcome the opportunity to present this project further and answer any questions the Board may have.

Sincerely,
Dean Behrend
Owner

NOTES:

- SUBJECT PARCEL IS SHOWN AS ASSESSORS MAP 124, LOT 17. RECORD TITLE FROM L.C. BOOK 601, PAGE 146.
- UTILITY LOCATIONS DEPICTED ON THIS PLAN, BOTH ABOVE- AND BELOW-GROUND, ARE BASED UPON DIRECT FIELD OBSERVATIONS MADE BY METROWEST ENGINEERING, INC. PERSONNEL DURING A FIELD SURVEY, RECORD PLAN LOCATIONS, OR DIGSAFE INDICATORS. METROWEST ENGINEERING, INC. DOES NOT WARRANT THAT ALL UTILITIES ARE SHOWN OR THAT UTILITIES THAT ARE DEPICTED ARE SHOWN IN THE CORRECT LOCATION, OR WITH THE PROPER MATERIAL DESIGNATION. METROWEST ENGINEERING, INC. DOES NOT WARRANT OR PROVIDE AN EXPRESS OR IMPLIED WARRANTY THAT ALL SUBSURFACE IMPROVEMENTS ARE SHOWN OR ARE SHOWN CORRECTLY, INCLUDING, BUT NOT LIMITED TO, UTILITIES, UNDERGROUND VAULTS, UNDERGROUND TANKS OR CHAMBERS, BUNKERS, DUCT BANKS, AND/OR OTHER MAN-MADE IMPROVEMENTS THAT LIE BEHIND THE GROUND SURFACE AT THE TIME OF THE SURVEY.
- CONTRACTOR IS SOLELY RESPONSIBLE FOR ESTABLISHING EXISTING LOCATIONS OF ALL SUB-SURFACE UTILITIES AND MAN-MADE IMPROVEMENTS AND FOR THE REQUIREMENTS TO REPLACE, RELOCATE OR REPAIR EXISTING UTILITIES IN THE EVENT OF DAMAGE OCCURRING DURING CONSTRUCTION. MWE IS NOT RESPONSIBLE OR LIABLE FOR DELAYS OR COSTS ASSOCIATED WITH REMOVING/REPLACING/RELOCATING OF EXISTING UTILITIES REGARDLESS OF WHETHER SAID UTILITIES ARE ACCURATELY DEPICTED ON THIS SURVEY.
- THE PROPERTY DESCRIBED ON THIS SURVEY DOES NOT LIE WITHIN A SPECIAL FLOOD HAZARD AREA AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY; THE PROPERTY LIES WITHIN ZONE "X" OF THE FLOOD INSURANCE RATE MAP IDENTIFIED AS MAP NUMBER 25021C0016E, BEARING AN EFFECTIVE DATE OF JULY 17, 2012.

DIG-SAFE
CONTRACTOR TO VERIFY ACTUAL LOCATION OF
EXISTING UTILITY SERVICES IN THE FIELD PRIOR TO
CONSTRUCTION (WATER, ELECTRICAL, ETC.)
CALL DIG-SAFE BEFORE YOU DIG 811.

LEGEND

A.C.	AIR CONDITIONER
B.H.	BULKHEAD
BIT.	BITUMINOUS
C.C.B.	CAPE COD BERM
CONC.	CONCRETE
C.P.	CONCRETE PAD
D.C.B.	DRAIN CATCH BASIN
D.M.H.	DRAIN MANHOLE
D.S.	DOWNSPOUT
E.H.H.	ELECTRIC HANDHOLE
E.M.	ELECTRIC METER
E.M.H.	ELECTRIC MANHOLE
E.O.D.	EDGE OF DRIVE
(F)	FOUND
G.G.	GAS GATE
G.M.	GAS METER
INV.	INVERT
I.R.	IRON ROD
L=	LENGTH
L.S.	LANDSCAPE
N/F	NOW OR FORMERLY
O.H.W.	OVERHEAD WIRE
R=	RADIUS
S.F.	SQUARE FEET
S.M.H.	SEWER MANHOLE
T.H.H.	TELEPHONE HANHOLE
U.G.D.S.	UNDERGROUND DOWNSPOUT
U.P.	UTILITY POLE
V.G.C.	VERTICAL GRANITE CURB
W.G.	WATER GATE
W.G.	WATERLINE
Δ=	DELTA
+100.0	NUMBER OF PARKING SPACES
-100-	EXISTING SPOT GRADE
-100-	EXISTING GRADING
GAS	GASLINE
U.G.	UNDERGROUND ELECTRIC LINE
W	WATERLINE

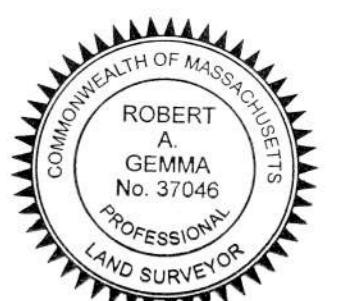
PLAN BOOK 215, PAGE 518

EXISTING TREE DESCRIPTION LEGEND

CODE	DESCRIPTION
BL	BLACK LOCUST
EB	EUROPEAN BEECH
HCN	HORSE CHESTNUT
HL	HONEY LOCUST
NM	NORWAY MAPLE
RO	RED OAK
SM	SUGAR MAPLE
DBL	DOUBLE

TREE LOCATION
WP06
TREE CODE
TREE DIAMETER

T.B.M.	DESCRIPTION	ELEVATION
A	"X" CUT ON HYDRANT	154.33'
B	NE CORNER RETAINING WALL STEPS	156.57'
C	NAIL SET 1' UP IN 20" SUGAR MAPLE	156.64'
D	NAIL SET 1' UP IN 16" RED OAK	155.73'
E	SE CORNER TOP OF STEP	154.94'
F	NAIL SET ON ROOT OF A STUMP	153.28'
G	NE CORNER OF STEP	154.97'
H	NAIL SET 1' UP ON U.P. #5-1/2	155.57'



FOR METROWEST ENGINEERING, INC. DATE
ROBERT A. GEMMA, P.L.S. # 37046
GRAPHIC SCALE
1 inch = 20 ft.
20 0 10 20 40 (FEET)
0 4 10 20 60 (METERS)

EXISTING CONDITIONS SITE PLAN #592 WASHINGTON STREET IN WELLESLEY, MASS (NORFOLK COUNTY)

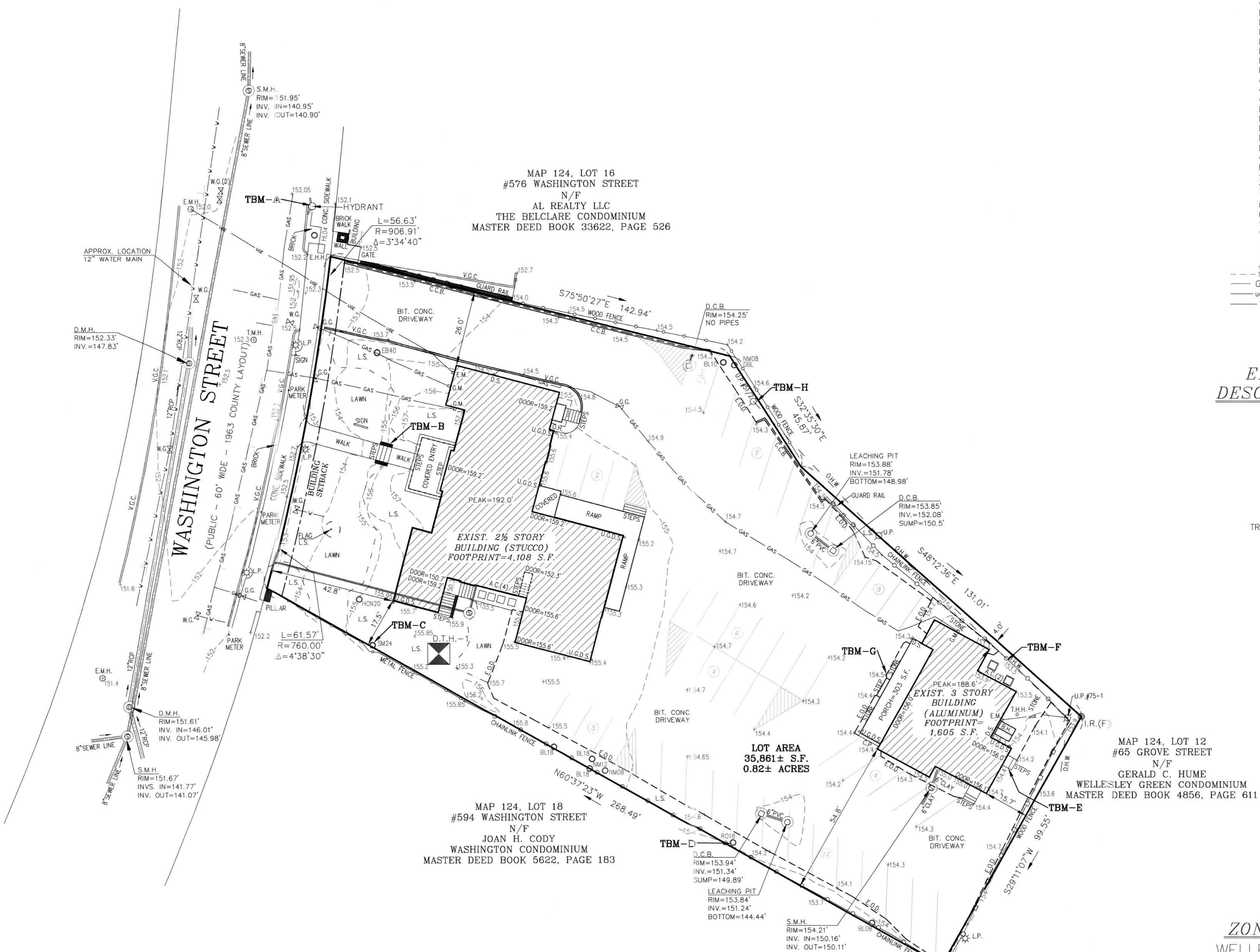
PREPARED FOR:
LYX/GROUP
96 ROCKWOOD STREET
BOSTON, MA 02130

PROPERTY OF:
THE WATERMAN REALTY MANAGEMENT LIMITED PARTNERSHIP
592A WASHINGTON STREET
WELLESLEY, MA 02482

ENGINEERS &
SURVEYORS:
MWE METROWEST ENGINEERING, INC.
75 FRANKLIN STREET
FRAMINGHAM, MA 01702
TELE: (508)626-0063
EMAIL: INFO@MWENGINEERING.COM

SHEET 1 OF 1 DATE: JUNE 27, 2023
CALC'D BY: PHA/BTN FIELD BK: 753 CAD FILE: LYX_01_R1.dwg
DRAFTER: PHA/JTS PROJECT: WEL_WAS4 DWG FILE: SP062723_R1.dwg

SP062723_R1



ZONING:

WELLESLEY SQUARE
COMMERCIAL DISTRICT

MINIMUM AREA = N.A.

MINIMUM FRONTAGE = N.A.

SETBACKS:

FRONT YARD = 5 FEET

SIDE YARD = N.A.

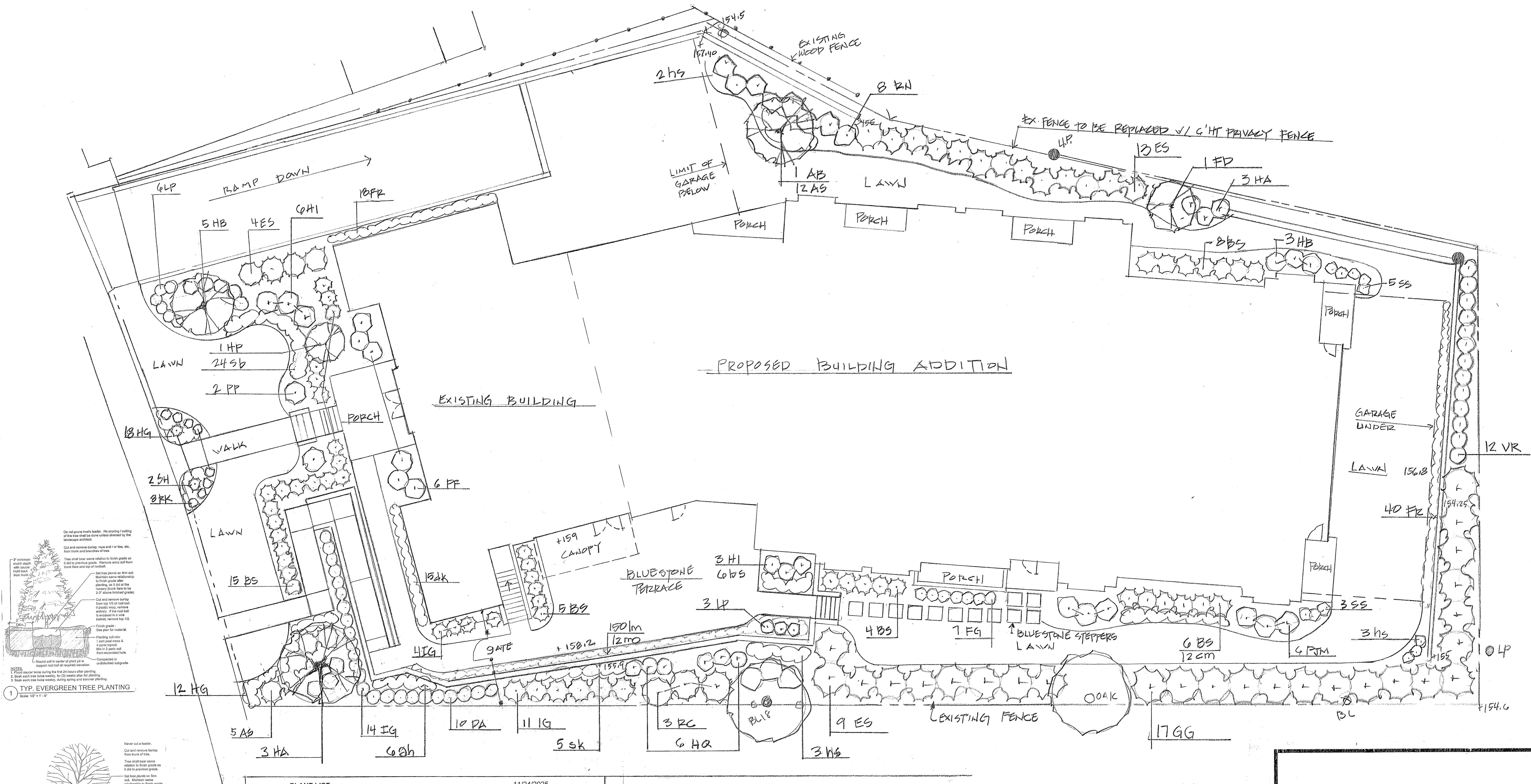
REAR YARD = N.A.

MAXIMUM HEIGHT = 45 FEET (3 STORIES)

MAXIMUM LOT COVERAGE = N.A.

FLOOR AREA RATIO = 0.30

NO.002.DWG

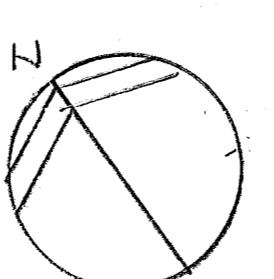


LANDSCAPE PLAN

PREPARED FOR:
592 WASHINGTON LLC
869 WORCESTER STREET
WELLESLEY, MA 02482

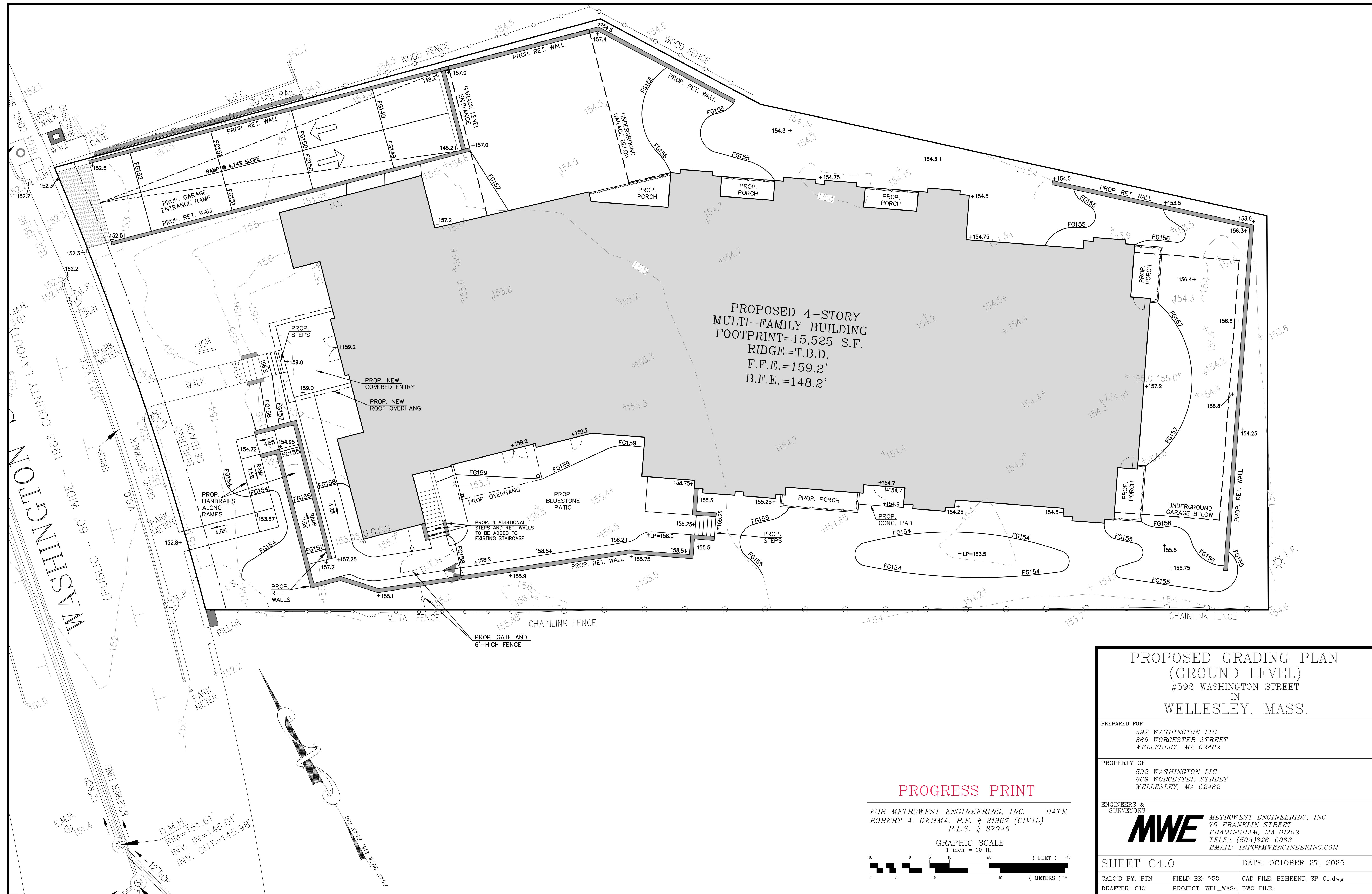
PROPERTY OF:
592 WASHINGTON LLC
869 WORCESTER STREET
WELLESLEY, MA 02482

Steven G. Cosmos
Registered Landscape Architect
Cosmosla33@gmail.com
508.654-6847



GRAPHIC SCALE
1 inch = 10 ft.
0 5 10 20 (FEET)
5 10 (METERS) 15

The diagram illustrates a graphic scale. It features a horizontal line with tick marks and numerical labels. The top part is labeled 'GRAPHIC SCALE' and '1 inch = 10 ft.'. Below this, the line has tick marks at 0, 5, 10, and 20, with the label '(FEET)' to the right. The bottom part has tick marks at 5 and 10, with the label '(METERS)' to the right. The segments between the tick marks are alternating black and white, with the segments starting from 0 being black.



NOTES:

1. SUBJECT PARCEL IS SHOWN AS ASSESSORS MAP 124, LOT 17. RECORD TITLE FROM CERTIFICATE #214101.

2. UTILITY LOCATIONS DEPICTED ON THIS PLAN, BOTH ABOVE- AND BELOW-GROUND, ARE BASED UPON DIRECT FIELD OBSERVATIONS MADE BY METROWEST ENGINEERING, INC. PERSONNEL DURING A FIELD SURVEY. RECORD PLAN LOCATIONS, OR DIGSAFE PAINT-INDICATORS. METROWEST ENGINEERING, INC. DOES NOT WARRANT THAT ALL UTILITIES ARE SHOWN OR THAT UTILITIES THAT ARE DEPICTED ARE SHOWN IN THE CORRECT LOCATION, OR WITH THE PROPER MATERIAL DESIGNATION. METROWEST ENGINEERING, INC. DOES NOT WARRANT OR PROVIDE AN EXPRESS OR IMPLIED WARRANTY THAT ALL SUBSURFACE IMPROVEMENTS ARE SHOWN OR ARE SHOWN CORRECTLY, INCLUDING, BUT NOT LIMITED TO, UTILITIES, UNDERGROUND VAULTS, UNDERGROUND TANKS OR CHAMBERS, BUNKERS, DUCT BANKS, AND/OR OTHER MAN-MADE IMPROVEMENTS THAT LIE BEHIND THE GROUND SURFACE AT THE TIME OF THE SURVEY.

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4. THE PROPERTY DESCRIBED ON THIS SURVEY DOES NOT LIE WITHIN A SPECIAL FLOOD HAZARD AREA AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY; THE PROPERTY LIES WITHIN ZONE "X" OF THE FLOOD INSURANCE RATE MAP IDENTIFIED AS MAP NUMBER 25021C0016F, BEARING AN EFFECTIVE DATE OF JULY 8, 2025.

 CONTRACTOR TO VERIFY ACTUAL LOCATION OF EXISTING UTILITY SERVICES IN THE FIELD PRIOR TO CONSTRUCTION (WATER, ELECTRICAL, ETC.) CALL DIG-SAFE BEFORE YOU DIG 811.

EXISTING TREE DESCRIPTION LEGEND

CODE	DESCRIPTION
BL	BLACK LOCUST
EB	EUROPEAN BEECH
H.C.N.	HORSE CHESTNUT
HL	HONEY LOCUST
N.M.	NORWAY MAPLE
RO	RED OAK
SM	SUGAR MAPLE
DBL	DOUBLE
TREE LOCATION	
WP06	
TREE CODE	
	TREE DIAMETER

LEGEND

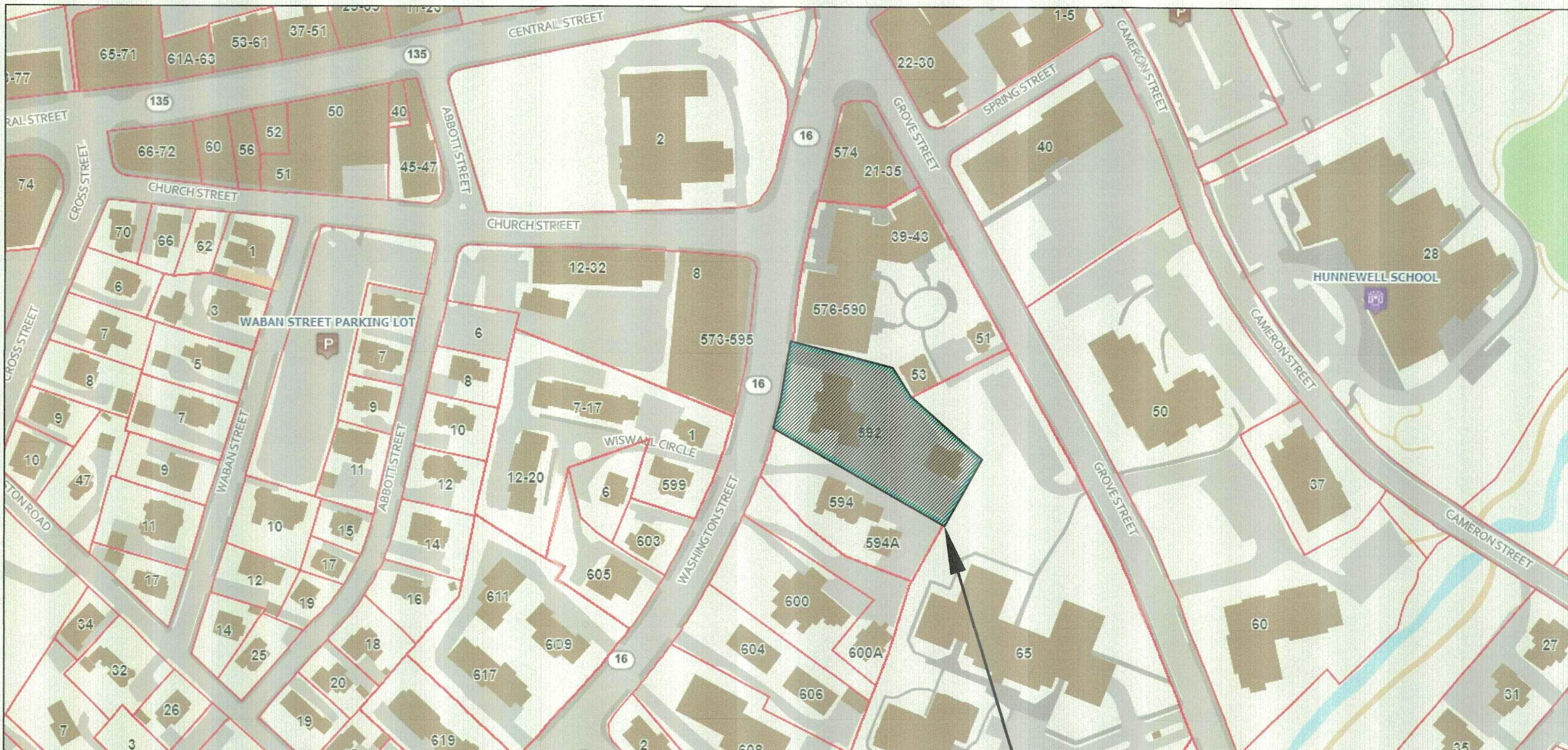
A.C.	AIR CONDITIONER
B.H.	BULKHEAD
BIT.	BITUMINOUS
C.C.B.	CAPE COD BERM
CONC.	CONCRETE
C.P.	CONCRETE PAD
D.C.B.	DRAIN CATCH BASIN
D.M.H.	DRAIN MANHOLE
D.S.	DOWNSPOUT
E.H.H.	ELECTRIC HANDHOLE
F.M.	ELECTRIC METER
F.M.H.	ELECTRIC MANHOLE
E.O.D.	EDGE OF DRIVE
(F)	FOUND.
G.G.	GAS GATE
G.M.	GAS METER
INV.	INVERT
I.R.	IRON ROD
L=	LENGTH
L.S.	LANDSCAPE
N/F	NOW OR FORMERLY
O.H.W.	OVERHEAD WIRE
R=	RADIUS
S.F.	SQUARE FEET
S.M.H.	SEWER MANHOLE
T.H.H.	TELEPHONE HANDHOLE
U.G.D.S.	UNDERGROUND DOWNSPOUT
U.P.	UTILITY POLE
V.G.C.	VERTICAL GRANITE CURB
W.G.	WATER GATE
Δ=	DELTA
(2)	NUMBER OF PARKING SPACES
+100.0	EXISTING SPOT GRADE
100-	EXISTING GRADE
— GAS	GASLINE
— USE	UNDERGROUND ELECTRIC LINE
W	WATERLINE

PROPOSED PARKING REQUIREMENTS

PARKING USE	PARKING REQUIREMENT	QUANTITY REQUIRED	QUANTITY PROPOSED
MULTI-UNIT HOUSING	2 SPACES PER DWELLING UNIT	38 SPACES	36 SPACES

1. PARKING REQUIREMENTS TAKEN FROM SECTION 5.17.D.2 & TABLE 21.1 OF CITY OF WELLESLEY ZONING BYLAWS

PROPOSED SITE PLAN SET
592 WASHINGTON STREET
WELLESLEY, MASSACHUSETTS



PROJECT SITE

SHEET INDEX

COVER SHEET	
SHEET C1.0	EXISTING CONDITIONS SITE PLAN
SHEET C2.0	EROSION AND SEDIMENT CONTROL PLAN
SHEET C3.0	PROPOSED LAYOUT PLAN
SHEET C4.0	PROPOSED GRADING PLAN (GROUND LEVEL)
SHEET C5.0	PROPOSED DRAINAGE PLAN (GROUND LEVEL)
SHEET C6.0	PROPOSED SITE PLAN (GROUND LEVEL)
SHEET C7.0	LOCUS PLAN
SHEET C8.0	DETAIL PLAN
SHEET C8.1	DETAIL PLAN

ZONING TABLE

WELLESLEY SQUARE COMMERCIAL DISTRICT

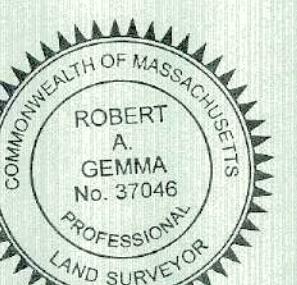
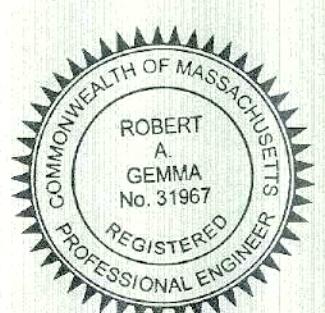
	REQUIRED	EXISTING	PROPOSED
AREA	N.A.	35,861± S.F.	35,861± S.F.
FRONTAGE	N.A.	118.20 FEET	118.20 FEET
SETBACKS:			
FRONT YARD	5 FEET	39.4 FEET (TO ENTRY)	34.9 FEET (TO ENTRY)
SIDE YARD	N.A.	42.8 FEET	42.8 FEET (TO BUILDING)
REAR YARD	N.A.	15.7 FEET	7.1 FEET (GARAGE)
BUILDING HEIGHT	3 STORIES	3 STORIES	3 STORIES
LOT COVERAGE:			
BUILDING	N.A.	5,709± S.F.	15,525± S.F.
IMPERVIOUS			
EXCLUDING BUILDINGS	N.A.	23,900± S.F.	5,130± S.F.
TOTAL IMPERVIOUS COVERAGE	N.A.	29,609± S.F.	20,655± S.F.
FLOOR AREA RATIO (FAR)	0.30 MAXIMUM	0.16	1.08

BENCHMARKS

ELEVATIONS SHOWN ARE BASED ON THE TOWN OF WELLESLEY DATUM

T.B.M.	DESCRIPTION	ELEVATION
A	"X" CUT ON HYDRANT	154.33'
B	NE CORNER RETAINING WALL STEPS	156.57'
C	NAIL SET 1' UP IN 20" SUGAR MAPLE	156.64'
D	NAIL SET 1' UP IN 16" RED OAK	155.73'
E	SE CORNER TOP OF STEP	154.94'
F	NAIL SET ON ROOT OF A STUMP	153.28'
G	NE CORNER OF STEP	154.97'
H	NAIL SET 1' UP ON U.P. #5-1/2	155.57'

PLAN BOOK 215, PLAN 518



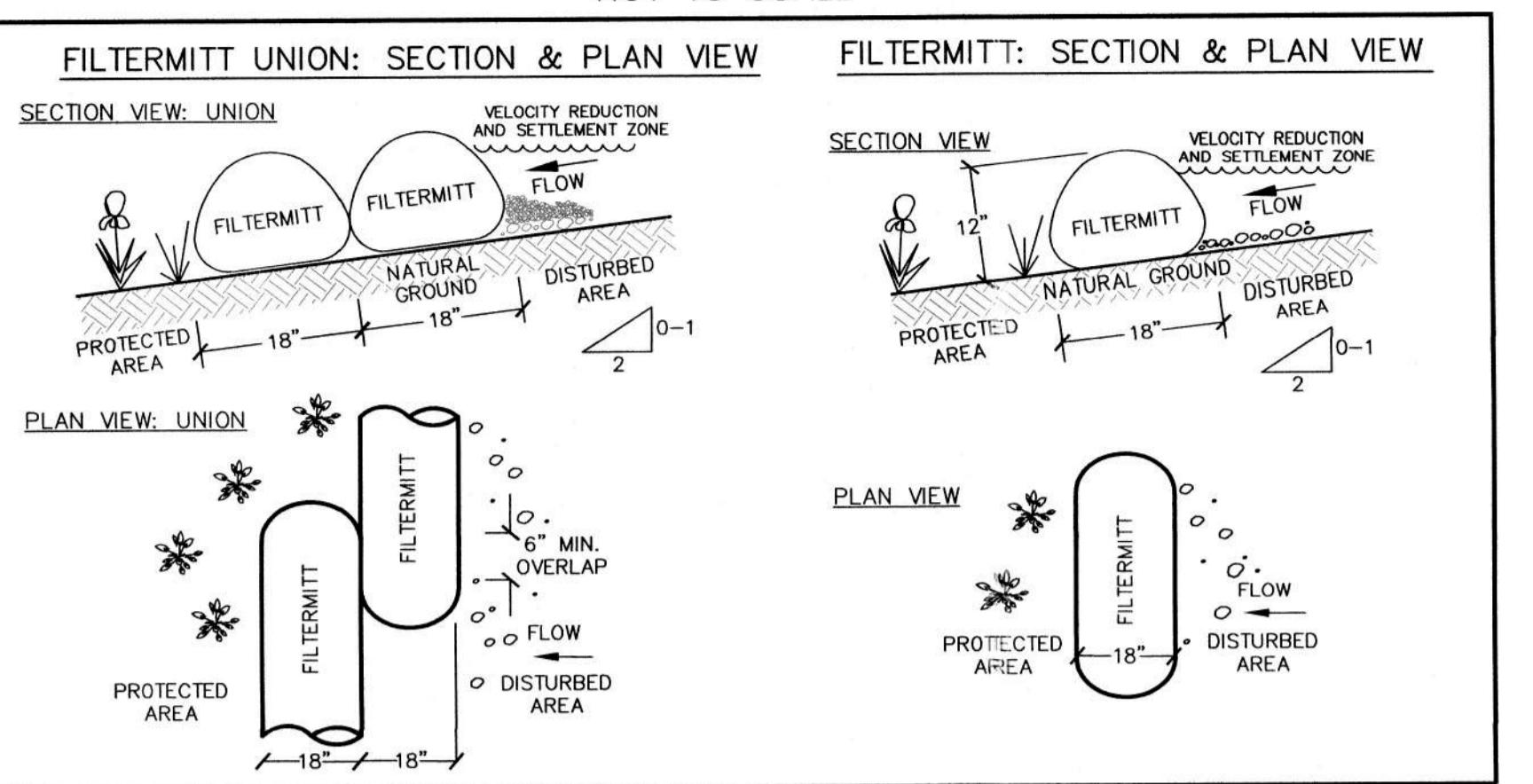
FOR METROWEST ENGINEERING, INC. DATE
ROBERT A. GEMMA, P.E.(CIVIL) # 31967
P.L.S. # 37046

COVER SHEET
#592 WASHINGTON STREET
IN
WELLESLEY, MASS
(NORFOLK COUNTY)

PREPARED FOR:	592 WASHINGTON LLC 869 WORCESTER STREET WELLESLEY, MA 02482
PROPERTY OF:	592 WASHINGTON LLC 869 WORCESTER STREET WELLESLEY, MA 02482
ENGINEERS & SURVEYORS:	MWE METROWEST ENGINEERING, INC. 75 FRANKLIN STREET FRAMINGHAM, MA 01702 TELE.: (508) 626-0063
COVER SHEET	DATE: NOVEMBER 24, 2025
CALC'D BY: BTN	FIELD BK: 753
DRAFTER: CJC	CAD FILE: BEHREND_SP_01.dwg
	DWG FILE: WEL_WAS4

FILTER MITT EROSION CONTROL BARRIER

NOT TO SCALE



LEGEND

A.C.	AIR CONDITIONER
B.H.	BULKHEAD
BIT.	BITUMINOUS
C.C.B.	CAPE COD BERM
CONC.	CONCRETE
C.P.	CONCRETE PAD
D.C.B.	DRAIN CATCH BASIN
D.M.H.	DRAIN MANHOLE
D.S.	DOWNSPOUT
E.H.H.	ELECTRIC HANDHOLE
E.M.	ELECTRIC METER
E.M.H.	ELECTRIC MANHOLE
E.O.D.	EDGE OF DRIVE
(F)	FOUND
G.G.	GAS GATE
G.M.	GAS METER
INV.	INVERT
I.R.	IRON ROD
L.	LENGTH
L.S.	LANDSCAPE
N/F	NOW OR FORMERLY
O.H.W.	OVERHEAD WIRE
R=	RADIUS
S.F.	SQUARE FEET
S.M.H.	SEWER MANHOLE
T.B.R.	TELEPHONE HANDHOLE
T.H.H.	UNDERGROUND DOWNSPOUT
U.G.D.S.	UTILITY POLE
U.P.	VERTICAL GRANITE CURB
V.G.C.	WATER GATE
W.C.	DELTA
W.G.	NUMBER OF PARKING SPACES
Δ=	EXISTING SPOT GRADE
(2)	EXISTING GRADING
100.0	GASLINE
100.0	UNDERGROUND ELECTRIC LINE
GAS	WATERLINE
use	
V	

NOTES:

- SUBJECT PARCEL IS SHOWN AS ASSESSORS MAP 124, LOT 17. RECORD TITLE FROM CERTIFICATE #214101.
- UTILITY LOCATIONS DEPICTED ON THIS PLAN, BOTH ABOVE- AND BELOW-GROUND, ARE BASED UPON DIRECT FIELD OBSERVATIONS MADE BY METROWEST ENGINEERING, INC. PERSONNEL DURING A FIELD SURVEY, RECORD PLAN LOCATIONS, OR DIGSAFE PAINT-INDICATORS. METROWEST ENGINEERING, INC. DOES NOT WARRANT THAT ALL UTILITIES ARE SHOWN OR THAT UTILITIES THAT ARE DEPICTED ARE SHOWN IN THE CORRECT LOCATION, OR WITH THE PROPER MATERIAL DESIGNATION. METROWEST ENGINEERING, INC. DOES NOT WARRANT THAT ALL IMPROVEMENTS ARE SHOWN OR ARE SHOWN CORRECTLY, INCLUDING, BUT NOT LIMITED TO, UTILITIES, UNDERGROUND VAULTS, UNDERGROUND TANKS OR CHAMBERS, BUNKERS, DUCT BANKS, AND/OR OTHER MAN-MADE IMPROVEMENTS THAT LIE BEHIND THE GROUND SURFACE AT THE TIME OF THE SURVEY.
- CONTRACTOR IS SOLELY RESPONSIBLE FOR ESTABLISHING EXISTING LOCATIONS OF ALL SUB-SURFACE UTILITIES AND MAN-MADE IMPROVEMENTS AND FOR THE REQUIREMENTS TO REPLACE, RELOCATE OR REPAIR EXISTING UTILITIES IN THE EVENT OF DAMAGE OCCURRING DURING CONSTRUCTION. MWE IS NOT RESPONSIBLE OR LIABLE FOR DELAYS OR COSTS ASSOCIATED WITH REMOVING/REPLACING/RELOCATING OF EXISTING UTILITIES REGARDLESS OF WHETHER SAID UTILITIES ARE ACCURATELY DEPICTED ON THIS SURVEY.
- THE PROPERTY DESCRIBED ON THIS SURVEY DOES NOT LIE WITHIN A SPECIAL FLOOD HAZARD AREA AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY; THE PROPERTY LIES WITHIN ZONE "X" OF THE FLOOD INSURANCE RATE MAP IDENTIFIED AS MAP NUMBER 25021C0016F, BEARING AN EFFECTIVE DATE OF JULY 8, 2025.

DIG-SAFE
CONTRACTOR TO VERIFY ACTUAL LOCATION OF EXISTING UTILITY SERVICES IN THE FIELD PRIOR TO CONSTRUCTION (WATER, ELECTRICAL, ETC.) CALL DIG-SAFE BEFORE YOU DIG 811.

BENCHMARKS

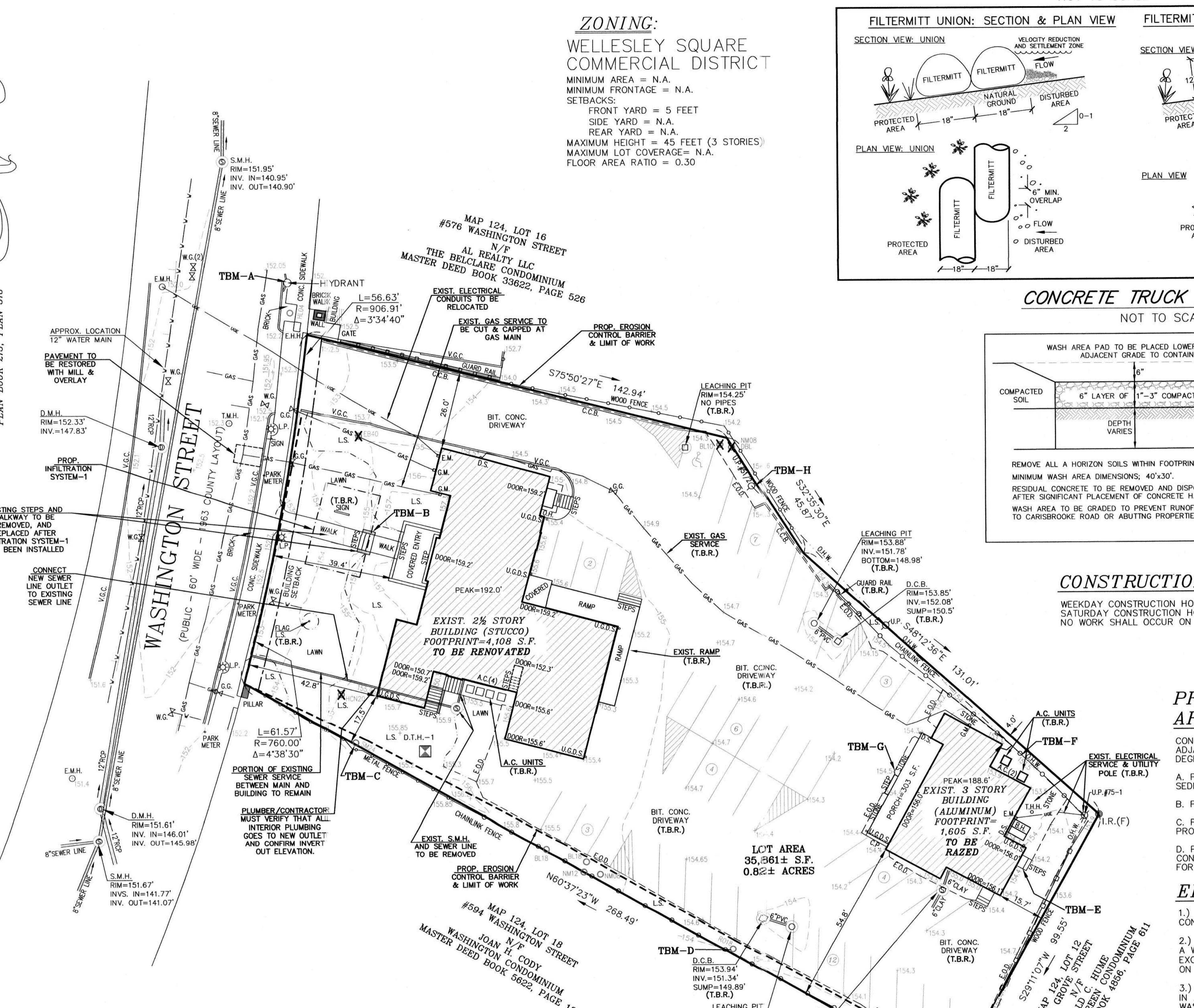
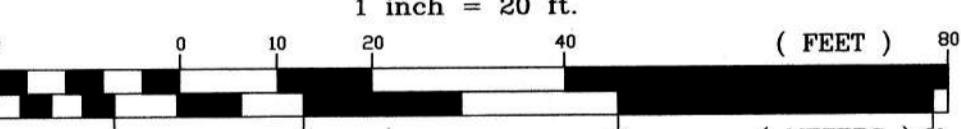
ELEVATIONS SHOWN ARE BASED ON THE TOWN OF WELLESLEY DATUM

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H	NAIL SET 1' UP ON U.P. #5-1/2	155.57'



FOR METROWEST ENGINEERING, INC. DATE
ROBERT A. GEMMA, P.E. # 31967 (CIVIL)
P.L.S. # 37046

GRAPHIC SCALE



OFF-SITE TRUCKING ROUTE

LARGE CONSTRUCTION VEHICLES SHALL EXIT THE SITE USING THE MOST DIRECT PATH TO WORCESTER ROAD (ROUTE 9):

EXIT THE SITE AND HEAD NORTH ON WASHINGTON STREET (ROUTE 16) TO THE INTERSECTION OF CENTRAL STREET AND WASHINGTON STREET.

PROCEED EAST ON WASHINGTON STREET (ROUTE 16) 1.5-MILES TO THE EASTBOUND RAMP FOR WORCESTER STREET (ROUTE 9).

PROCEED EAST ON WORCESTER STREET TO THE WELLESLEY TOWN LINE.

OR;

PROCEED NORTH ON WASHINGTON STREET (ROUTE 16) 140-FEET TO THE INTERSECTION OF CHURCH STREET AND WASHINGTON STREET.

PROCEED WEST ON CHURCH STREET 935-FEET TO THE INTERSECTION OF CHURCH STREET AND CROSS STREET.

PROCEED SOUTH ON CROSS STREET TO THE INTERSECTION OF CROSS STREET AND WESTON ROAD.

PROCEED NORTH ON WESTON ROAD 1.2-MILES TO THE WESTBOUND RAMP FOR WORCESTER STREET (ROUTE 9).

PROCEED WEST ON WORCESTER STREET (ROUTE 9) TO WELLESLEY TOWN LINE.

SOIL TEST RESULTS	
D.T.H.-1 (ELEV=155.9')	
0"-16" A	SANDY LOAM 10YR 3/2
16"-36" B	SANDY LOAM 10YR 5/6
36"-72" C1	SAND 2.5Y 5/3
72"-120" C2	SAND 2.5Y 5/4
C1 HORIZON HAS 40% GRAVEL POSSIBLE MOTTLING @106' NO STANDING OR WEPPING WATER NO REFUSAL ESTIMATED HIGH WATER-BELOW 147.1'	
DATE: AUGUST 08, 2023	
BY: BRIAN NELSON, SOIL EVALUATOR F.B. 757, PAGE 02	
INSPECTOR: NONE	

PROTECTION OF STORMWATER MANAGEMENT AREAS DURING CONSTRUCTION

CONTRACTOR SHALL EXERCISE CARE TO PROTECT THE SURFACES BENEATH AND ADJACENT TO THE PROPOSED INFILTRATION SYSTEMS AND RAIN GARDEN FROM DEGRADATION BY SURROUNDING CONSTRUCTION ACTIVITIES, INCLUDING:

- PREVENTION OF CONTAMINATION OF EXPOSED SUBGRADE BY CONSTRUCTION SEDIMENT.
- PREVENTION OF EXCESSIVE COMPACTION BY CONSTRUCTION VEHICLES.
- PREVENTION OF DISCHARGE OF WATER FROM DEWATERING ACTIVITIES INTO THE PROPOSED INFILTRATION SYSTEM OR RAIN GARDENS.
- PREVENTION OF DISCHARGE OF STORMWATER INTO THESE FACILITIES UNTIL THE CONTRIBUTING AREAS ARE STABILIZED UNLESS SPECIFIC MEASURES ARE PROVIDED FOR PROTECTING AND RESTORING THE INFILTRATION SURFACE.

EROSION CONTROL NOTES:

- CONTRACTOR SHALL MODIFY SEDIMENTATION CONTROLS AS NECESSARY DURING CONSTRUCTION.
- CONTRACTOR SHALL INSPECT AND CLEAN ALL SILTATION CONTROL MEASURES ON A WEEKLY BASIS AND AFTER ALL STORMS WITH A PRECIPITATION AMOUNT IN EXCESS OF 1". CONTRACTOR SHALL IMMEDIATELY REPAIR ANY DEFICIENCIES FOUND ON SILTATION CONTROL MEASURES.
- CONTRACTOR SHALL MAINTAIN AN INVENTORY OF EMERGENCY SUPPLIES ON SITE IN A PROTECTED LOCATION INCLUDING, BUT NOT LIMITED TO HAYBALES, SILT FENCE, WASHED STONE, PVC PIPE, MULCH AND SEED.
- ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED BEFORE OR THROUGH THE WINTER TO ELIMINATE DESTABILIZATION OR SILTATION DURING THE SPRING THAW.
- ANY RELEASE OF WATER FROM TEMPORARY SILTATION PONDS SHALL BE FREE OF SILT AND SEDIMENT AND SHALL MEET WATER QUALITY STANDARDS FOR CLASS B WATER IN THE COMMONWEALTH OF MASSACHUSETTS.
- IF GROUNDWATER DE-WATERING IS REQUIRED, CONTRACTOR SHALL IMPLEMENT A FILTERING SYSTEM FOR PUMPED GROUNDWATER TO REMOVE SILT AND SEDIMENT. THE DIRECT DISCHARGE OF ANY DE-WATERING OPERATION INTO A WETLAND IS PROHIBITED.
- CONTRACTOR SHALL REMOVE AND PROPERLY DISPOSE OF ALL SEDIMENTS WHEN THEY HAVE ACCUMULATED TO A HEIGHT APPROXIMATELY EQUAL TO ONE HALF OF THE HEIGHT OF THE CORRESPONDING EROSION AND SEDIMENTATION CONTROL MEASURE.
- CONTRACTOR SHALL KEEP COPIES OF THE EROSION CONTROL PLAN AND INSPECTION REPORTS SO THEY ARE ACCESSIBLE AT ALL TIMES.
- CONTRACTOR TO PROTECT ALL DRAINAGE INLETS FROM SILT AND SEDIMENT UNTIL THEIR TRIBUTARY AREAS ARE FULLY STABILIZED.
- CONTRACTOR TO SWEEP STREET IN FRONT OF WORK AREA WEEKLY OR AS NEEDED TO PREVENT ACCUMULATION OF SILT AND SEDIMENT ON GARDEN ROAD.
- ANY DEWATERING PERFORMED ON SITE REQUIRING A CONNECTION OR DISCHARGE TO THE MUNICIPAL DRAINAGE SYSTEM MUST BE APPROVED BY THE TOWN ENGINEER PRIOR TO DISCHARGE.

EROSION & SEDIMENT CONTROL PLAN

#592 WASHINGTON STREET
IN
WELLESLEY, MASS
(NORFOLK COUNTY)

PREPARED FOR:
592 WASHINGTON LLC
869 WORCESTER STREET
WELLESLEY, MA 02482

PROPERTY OF:
592 WASHINGTON LLC
869 WORCESTER STREET
WELLESLEY, MA 02482

ENGINEERS &
SURVEYORS:
MWE METROWEST ENGINEERING, INC.
75 FRANKLIN STREET
FRAMINGHAM, MA 01702
TELE: (508)626-0063
EMAIL: INFO@MWEENGINEERING.COM

SHEET C2.0

DATE: NOVEMBER 24, 2025

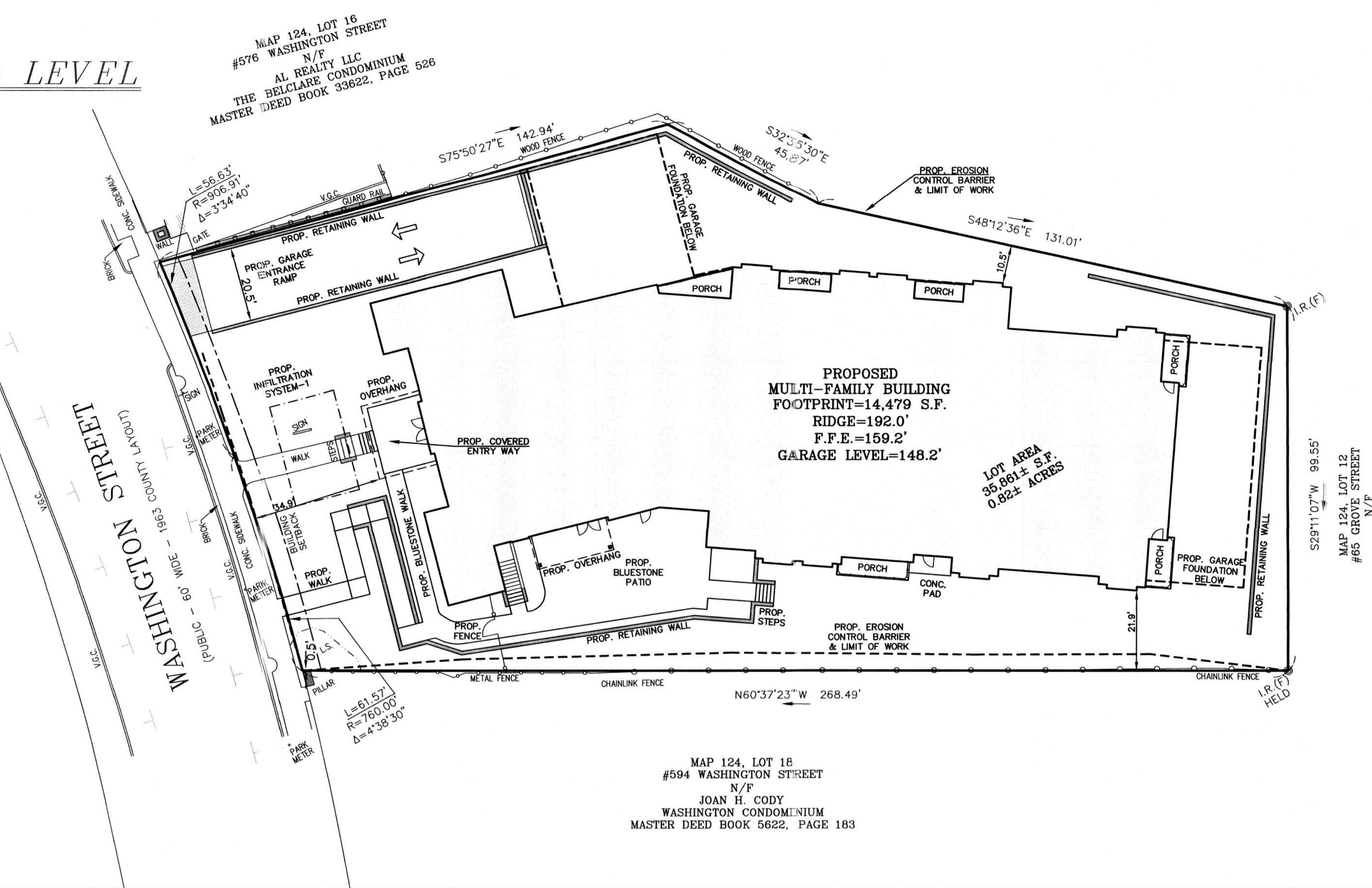
CALC'D BY: BTN

FIELD BK: 753

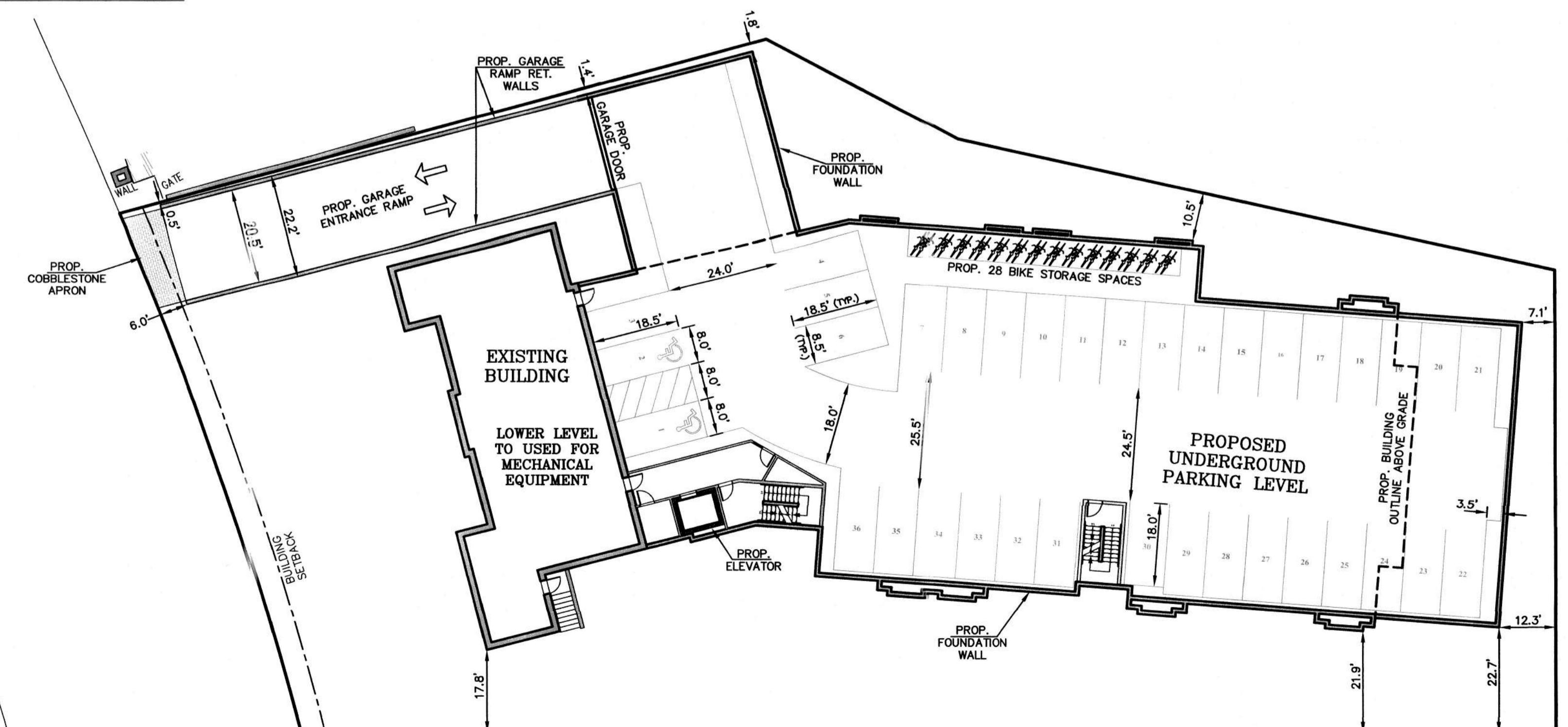
DRAFTER: CJC

CAD FILE: BEHREND_SP_01.dwg

GROUND LEVEL



BASEMENT LEVEL



ZONING:

WELLESLEY SQUARE COMMERCIAL DISTRICT

MINIMUM AREA = N.A.

MINIMUM FRONTAGE = N.A.

SETBACKS:

FRONT YARD = 5 FEET

SIDE YARD = N.A.

REAR YARD = N.A.

MAXIMUM HEIGHT = 45 FEET (3 STORIES)

MAXIMUM LOT COVERAGE= N.A.

FLOOR AREA RATIO = 0.30

IMPERVIOUS AREA TABLE

EXISTING BUILDINGS	= 6,346 SQ. FT.
EXISTING DRIVEWAY & PARKING LOT	= 20,558 SQ. FT.
EXISTING CONC. PADS, WALKWAYS & ETC.	= 1,065 SQ. FT.
TOTAL EXISTING IMPERVIOUS AREA = 27,969 SQ. FT.	
PROPOSED BUILDING	= 15,525 SQ. FT.
PROPOSED DRIVEWAY	= 2,032 SQ. FT.
PROPOSED PATIO & WALKWAYS	= 3,098 SQ. FT.
TOTAL PROPOSED IMPERVIOUS AREA = 20,655 SQ. FT.	



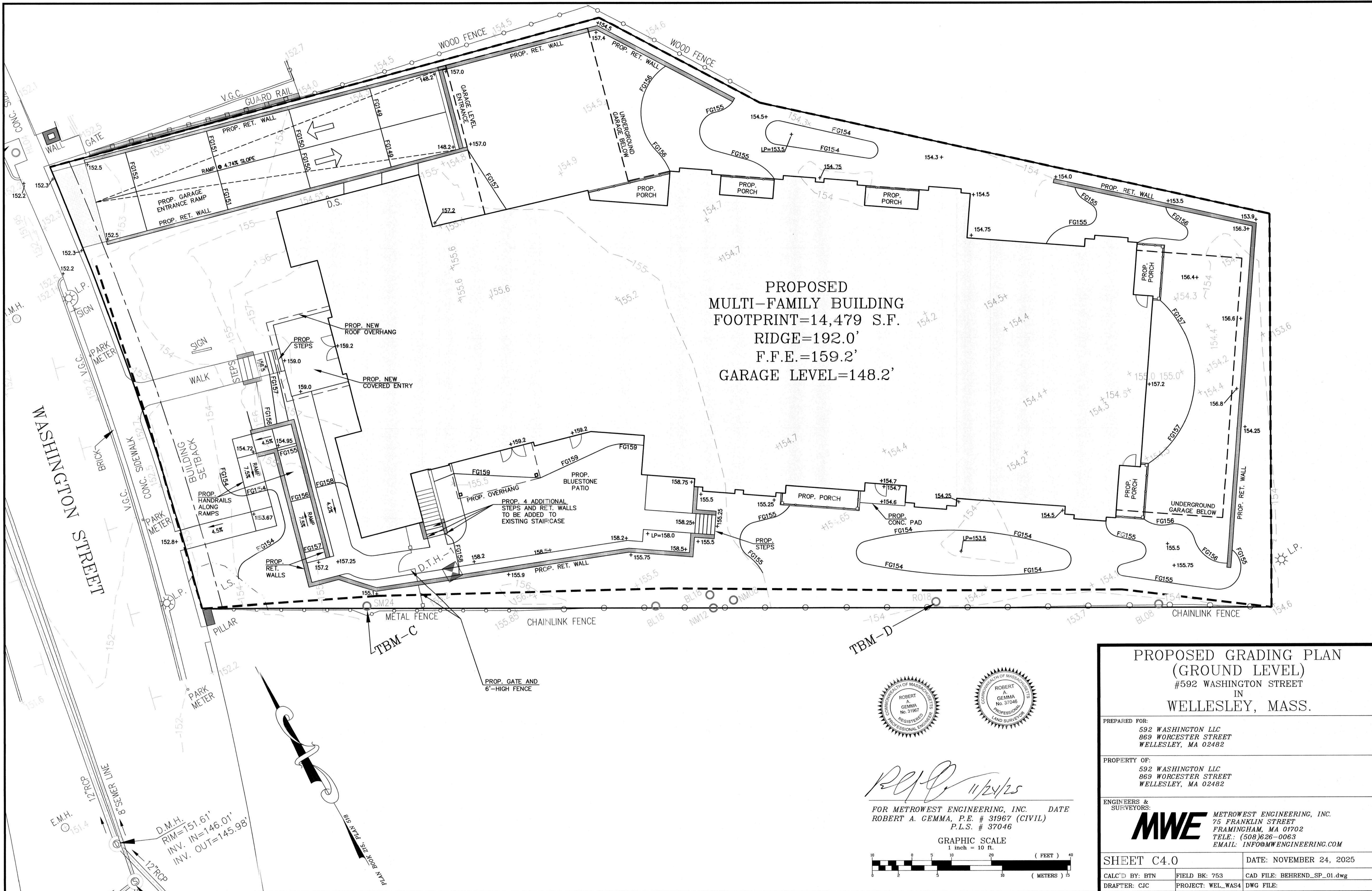
11/27/25
FOR METROWEST ENGINEERING, INC. DATE
ROBERT A. GEMMA, P.E. # 31967 (CIVIL)
P.L.S. # 37046

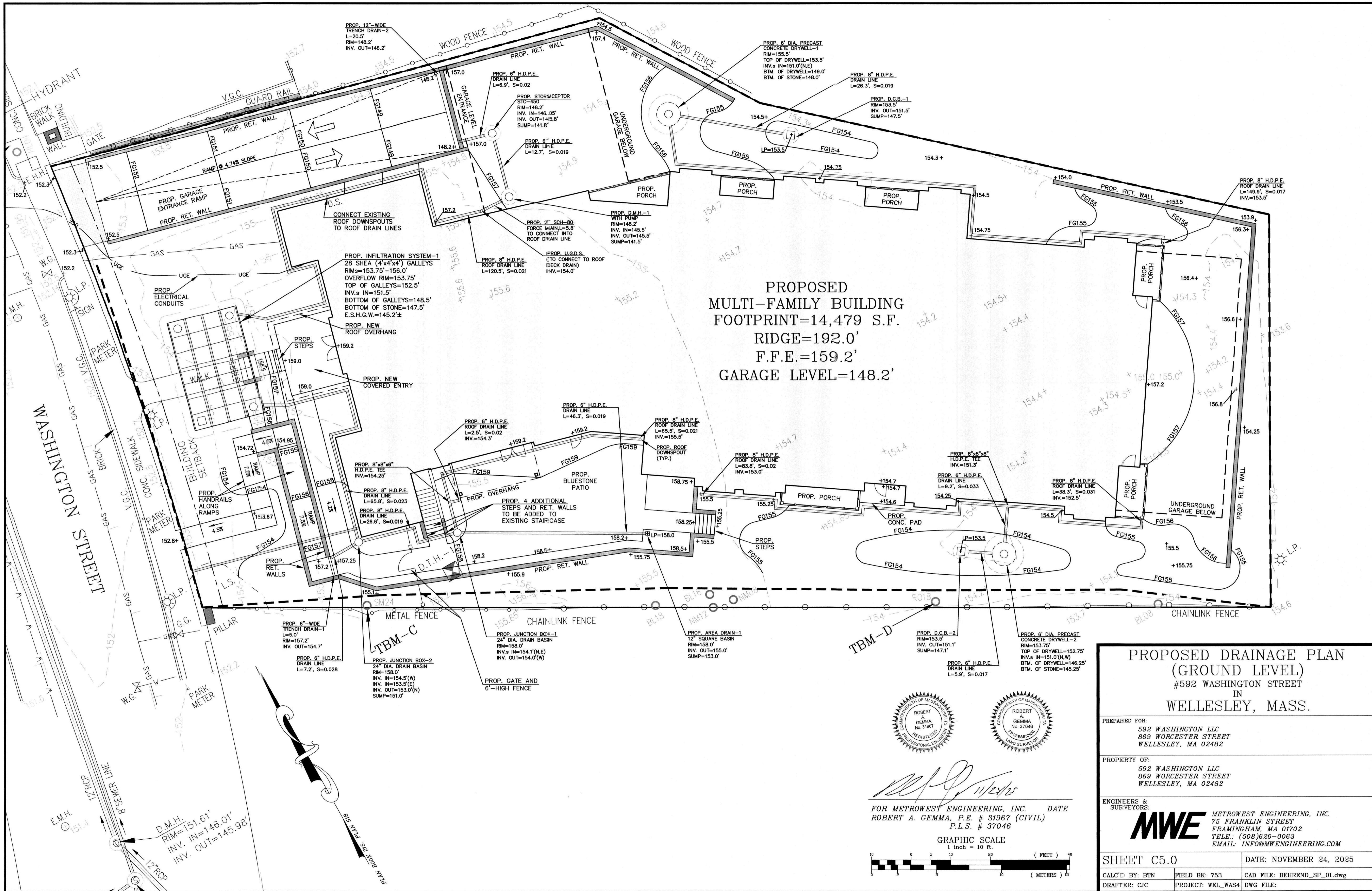
GRAPHIC SCALE
1 inch = 20 ft.
(FEET)
0 20 0 10 20 40 60
0 4 10 20 30 (METERS)

PROPOSED LAYOUT PLAN

#592 WASHINGTON STREET
IN
WELLESLEY, MASS.

PREPARED FOR:	592 WASHINGTON LLC 869 WORCESTER STREET WELLESLEY, MA 02482
PROPERTY OF:	592 WASHINGTON LLC 869 WORCESTER STREET WELLESLEY, MA 02482
ENGINEERS & SURVEYORS:	MWE METROWEST ENGINEERING, INC. 75 FRANKLIN STREET FRAMINGHAM, MA 01702 TELE: (508)626-0063 EMAIL: INFO@MWENGINEERING.COM
SHEET C3.0	DATE: NOVEMBER 24, 2025
CALC'D BY: BTN	FIELD BK: 753
DRAFTER: CJC	PROJECT: WEL_WAS4
	DWG FILE: DWG





NOTES:

- SUBJECT PARCEL IS SHOWN AS ASSESSORS MAP 124, LOT 17. RECORD TITLE FROM CERTIFICATE #214101.
- UTILITY LOCATIONS DEPICTED ON THIS PLAN, BOTH ABOVE- AND BELOW-GROUND, ARE BASED UPON DIRECT FIELD OBSERVATIONS MADE BY METROWEST ENGINEERING, INC. PERSONNEL DURING A FIELD SURVEY. RECORD PLAN LOCATIONS, OR DIGSAFE PAINT-INDICATORS. METROWEST ENGINEERING, INC. DOES NOT WARRANT THAT ALL UTILITIES ARE SHOWN, OR THAT UTILITIES THAT ARE DEPICTED ARE SHOWN IN THE CORRECT LOCATION, OR WITH THE PROPER MATERIAL DESIGNATION. METROWEST ENGINEERING, INC. DOES NOT WARRANT OR PROVIDE AN EXPRESS OR IMPLIED WARRANTY THAT ALL SUBSURFACE IMPROVEMENTS ARE SHOWN OR ARE SHOWN CORRECTLY, INCLUDING, BUT NOT LIMITED TO, UTILITIES, UNDERGROUND VAULTS, UNDERGROUND TANKS OR CHAMBERS, BUNKERS, DUCT BANKS, AND/OR OTHER MAN-MADE IMPROVEMENTS THAT LIE BEHIND THE GROUND SURFACE AT THE TIME OF THE SURVEY.
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LEGEND

A.C.	AIR CONDITIONER
B.H.	BULKHEAD
BIT.	BITUMINOUS
C.C.B.	CAPE COD BERM
CONC.	CONCRETE
C.P.	CONCRETE PAD
D.C.B.	DRAIN CATCH BASIN
D.M.H.	DRAIN MANHOLE
D.S.	DOWNSPOUT
E.H.H.	ELECTRIC HANDHOLE
E.M.	ELECTRIC METER
E.M.H.	ELECTRIC MANHOLE
E.O.D.	EDGE OF DRIVE
(F)	FOUND
G.G.	GAS GATE
G.M.	GAS METER
INV.	INVERT
I.R.	IRON ROD
L=	LENGTH
L.S.	LANDSCAPE
N/F	NOW OR FORMERLY
O.H.W.	OVERHEAD WIRE
R=	RADIUS
S.F.	SQUARE FEET
S.M.H.	SEWER MANHOLE
T.H.H.	TELEPHONE HANDHOLE
U.G.D.S.	UNDERGROUND DOWNSPOUT
U.P.	UTILITY POLE
V.G.C.	VERTICAL GRANITE CURB
W.G.	WATER GATE
Δ=	DELTA
+100.0	NUMBER OF PARKING SPACES
-100-	EXISTING SPOT GRADE
-100-	EXISTING GRADING
GAS	GASLINE
use	UNDERGROUND ELECTRIC LINE
W	WATERLINE

DIG-SAFE CONTRACTOR TO VERIFY ACTUAL LOCATION OF EXISTING UTILITY SERVICES IN THE FIELD PRIOR TO CONSTRUCTION (WATER, ELECTRICAL, ETC.) CALL DIG-SAFE BEFORE YOU DIG 811.

EXISTING TREE DESCRIPTION LEGEND

CODE	DESCRIPTION
BL	BLACK LOCUST
EB	EUROPEAN BEECH
HCN	HORSE CHESTNUT
HL	HONEY LOCUST
NM	NORWAY MAPLE
RO	RED OAK
SM	SUGAR MAPLE
DBL	DOUBLE

TREE LOCATION
TREE CODE
TREE DIAMETER



FOR METROWEST ENGINEERING, INC. DATE
ROBERT A. GEMMA, P.E. # 31967 (CIVIL)
P.L.S. # 37046

GRAPHIC SCALE
1 inch = 20 ft.
(FEET)
0 4 8 12 16 20 24 28 32 36 40 44 48 52 56 60
(METERS) 0 2 4 6 8 10 12 14 16 18 20 22 24 26 28 30

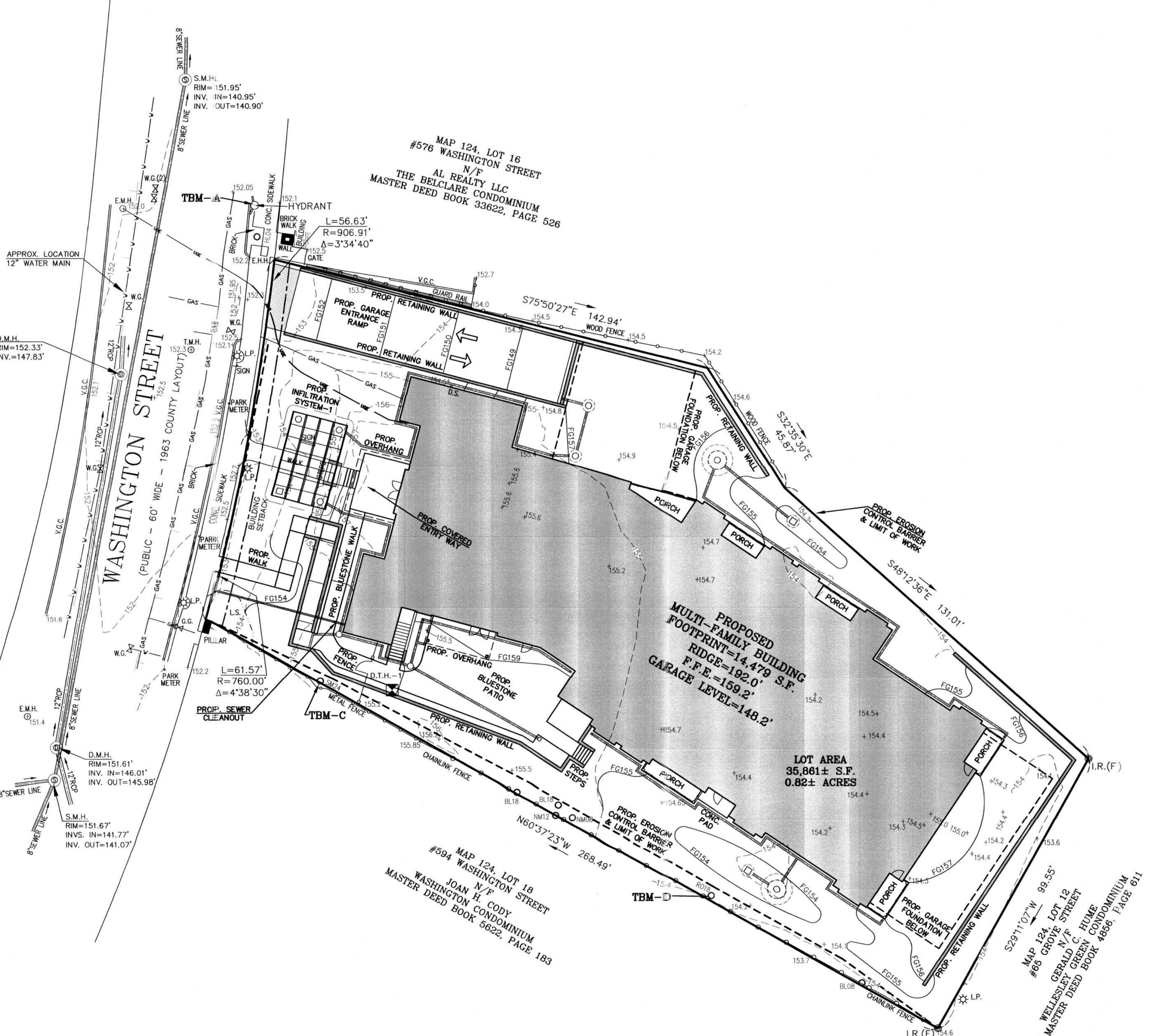
PROPOSED SITE PLAN (GROUND LEVEL) #592 WASHINGTON STREET IN WELLESLEY, MASS.

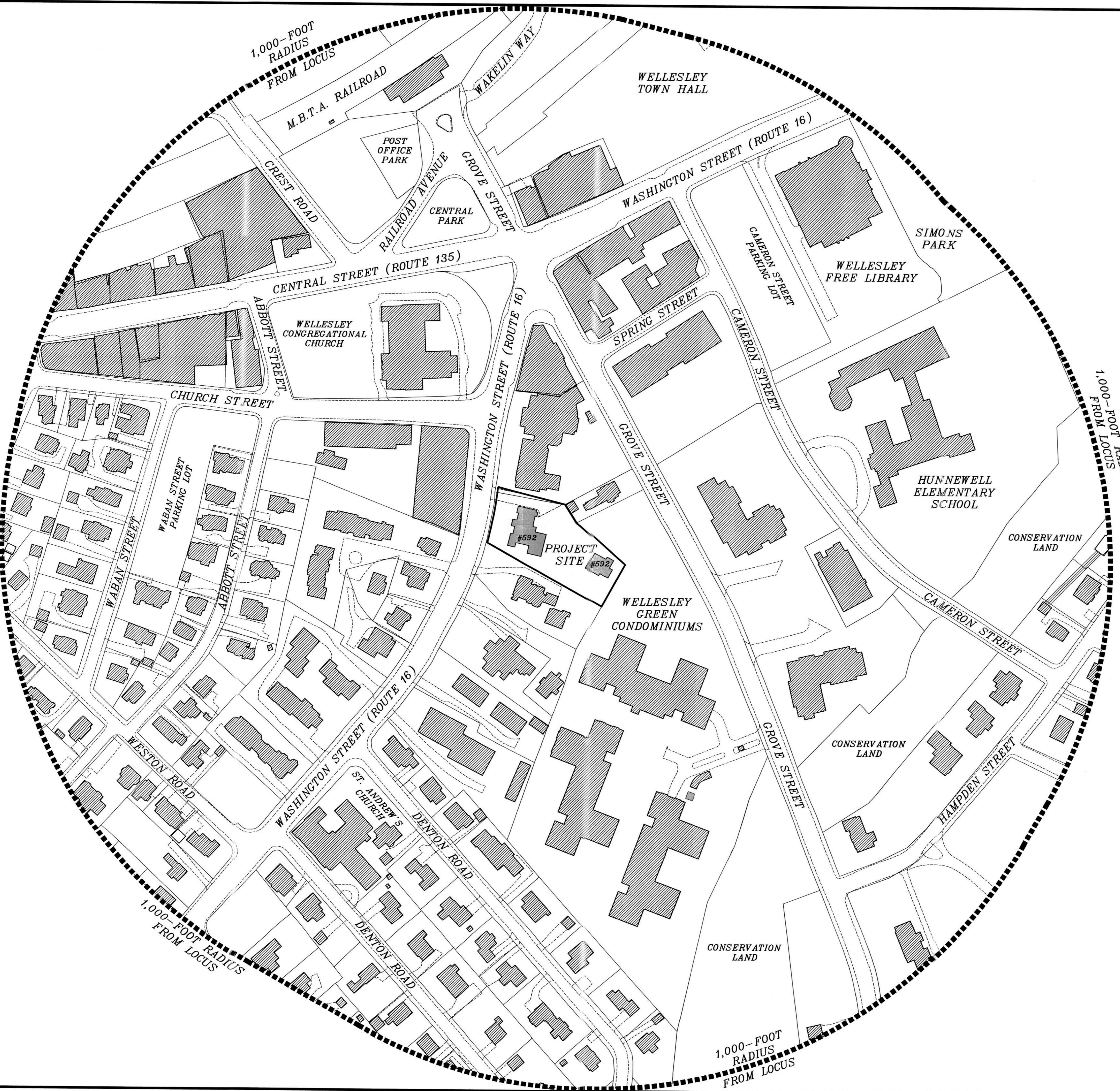
PREPARED FOR:
592 WASHINGTON LLC
869 WORCESTER STREET
WELLESLEY, MA 02482

PROPERTY OF:
592 WASHINGTON LLC
869 WORCESTER STREET
WELLESLEY, MA 02482

ENGINEERS &
SURVEYORS:
MWE METROWEST ENGINEERING, INC.
75 FRANKLIN STREET
FRAMINGHAM, MA 01702
TELE: (508)626-0063
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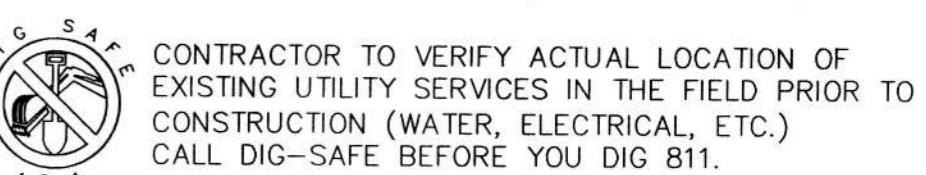
SHEET C6.0 DATE: NOVEMBER 24, 2025
CALC'D BY: BTN FIELD BK: 753 CAD FILE: BEHREND_SP_01.dwg
DRAFTER: CJC PROJECT: WEL_WAS4 DWG FILE:





NOTES:

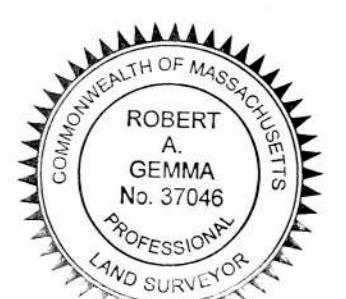
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ZONING:

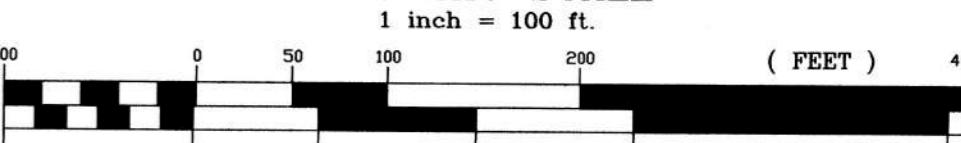
WELLESLEY SQUARE COMMERCIAL DISTRICT

MINIMUM AREA = N.A.
MINIMUM FRONTAGE = N.A.
SETBACKS:
FRONT YARD = 5 FEET
SIDE YARD = N.A.
REAR YARD = N.A.
MAXIMUM HEIGHT = 45 FEET (3 STORIES)
MAXIMUM LOT COVERAGE = N.A.
FLOOR AREA RATIO = 0.30



FOR METROWEST ENGINEERING, INC. DATE
ROBERT A. GEMMA, P.L.S. # 37046

GRAPHIC SCALE

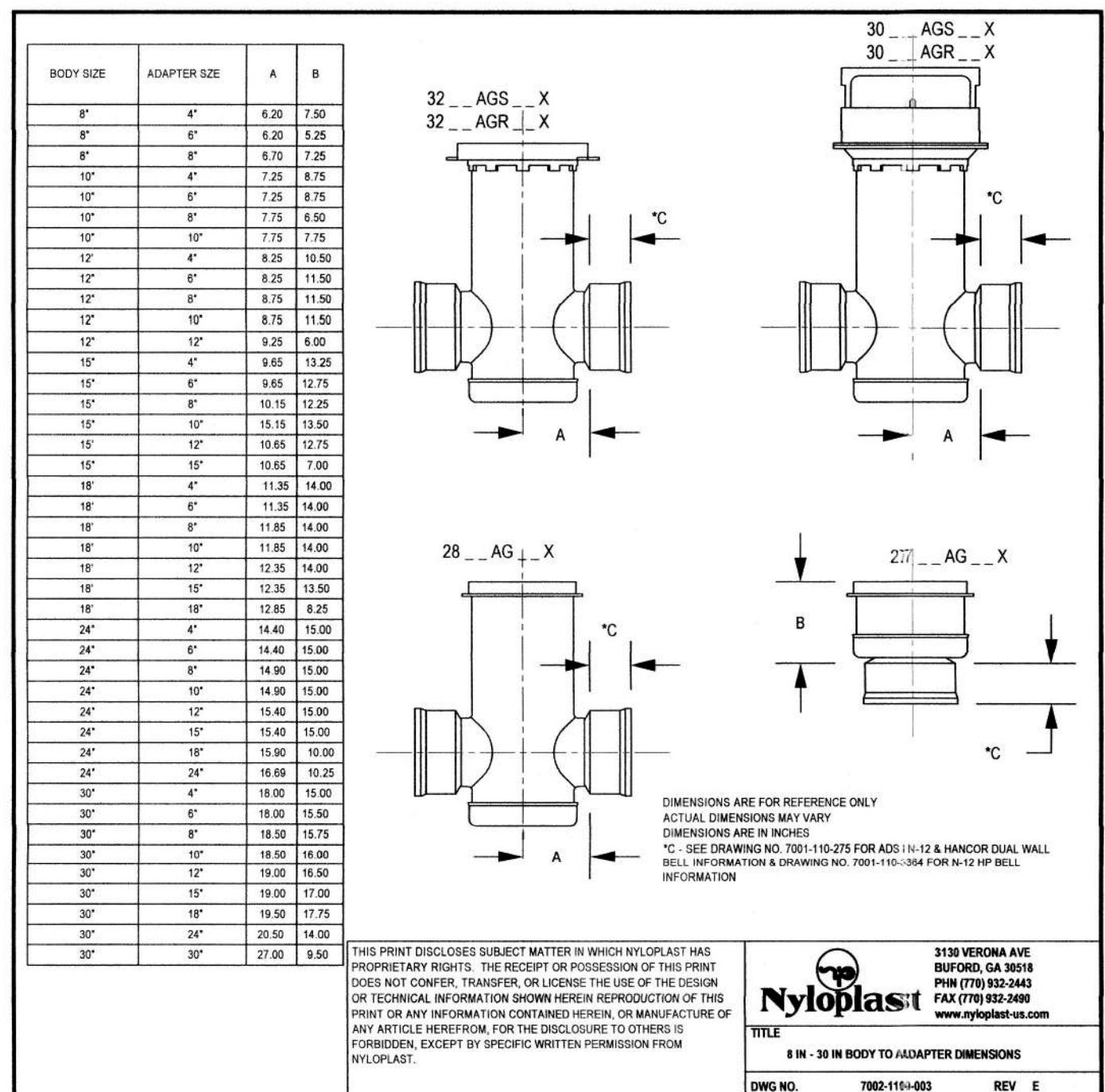


LOCUS PLAN

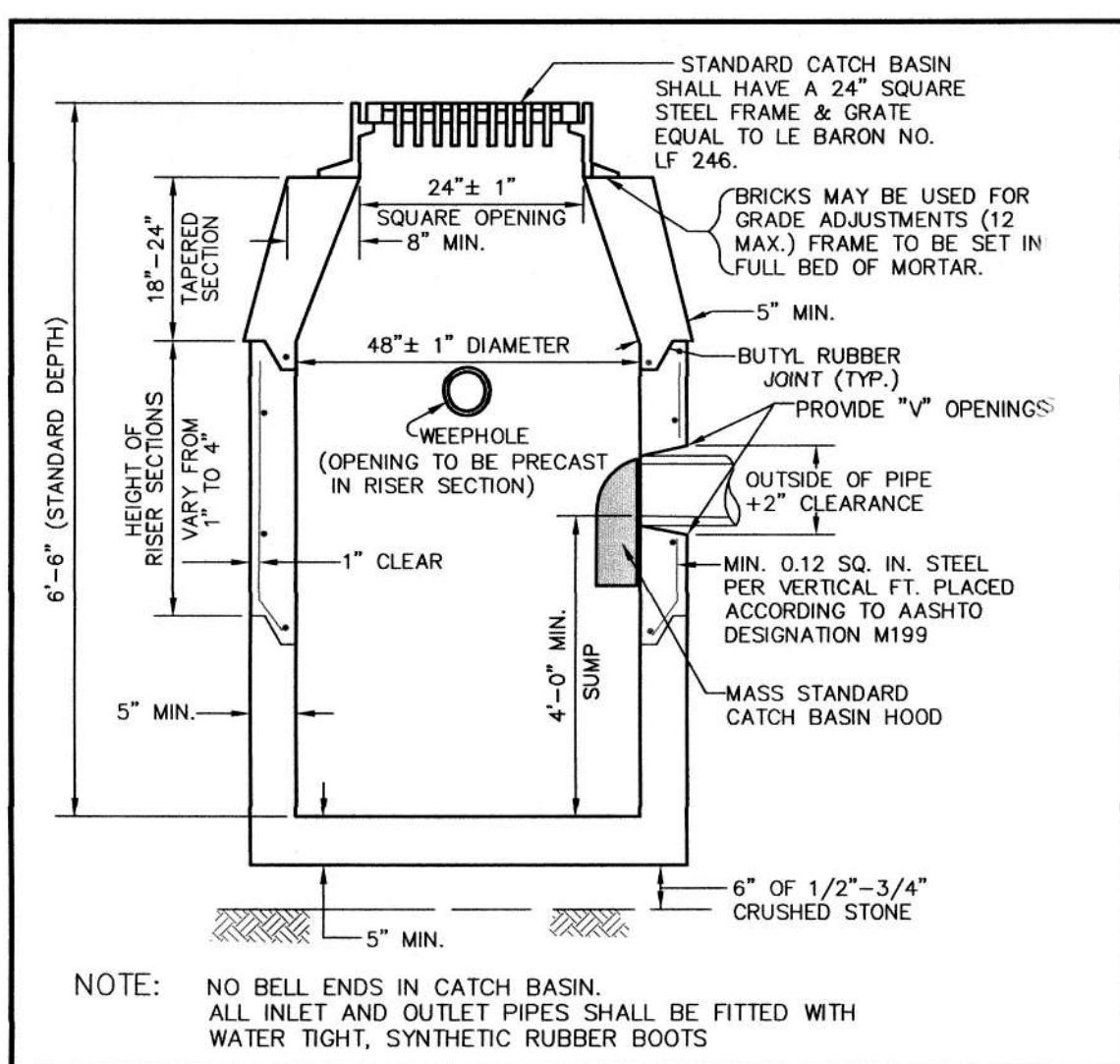
#592 WASHINGTON STREET
IN
WELLESLEY, MASS
(NORFOLK COUNTY)

PREPARED FOR:	592 WASHINGTON LLC 869 WORCESTER STREET WELLESLEY, MA 02482
PROPERTY OF:	592 WASHINGTON LLC 869 WORCESTER STREET WELLESLEY, MA 02482
ENGINEERS & SURVEYORS:	MWE METROWEST ENGINEERING, INC. 75 FRANKLIN STREET FRAMINGHAM, MA 01702 TELE: (508)626-0063
SHEET C7.0	DATE: NOVEMBER 24, 2025
CALC'D BY: BTN	FIELD BK: 753
DRAFTER: CJC	CAD FILE: BEHREND_SP_01.dwg PROJECT: WEL_WAS4 DWG FILE:

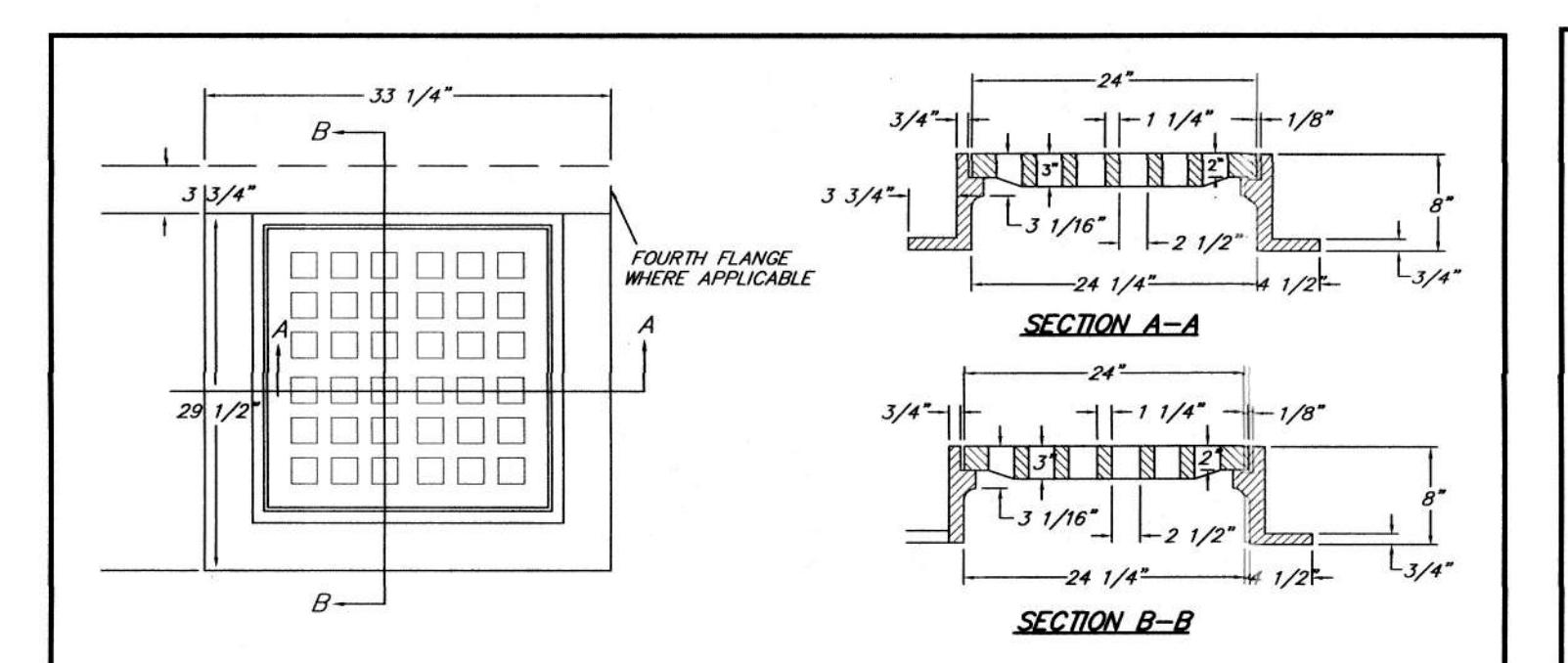
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NOT TO SCALE



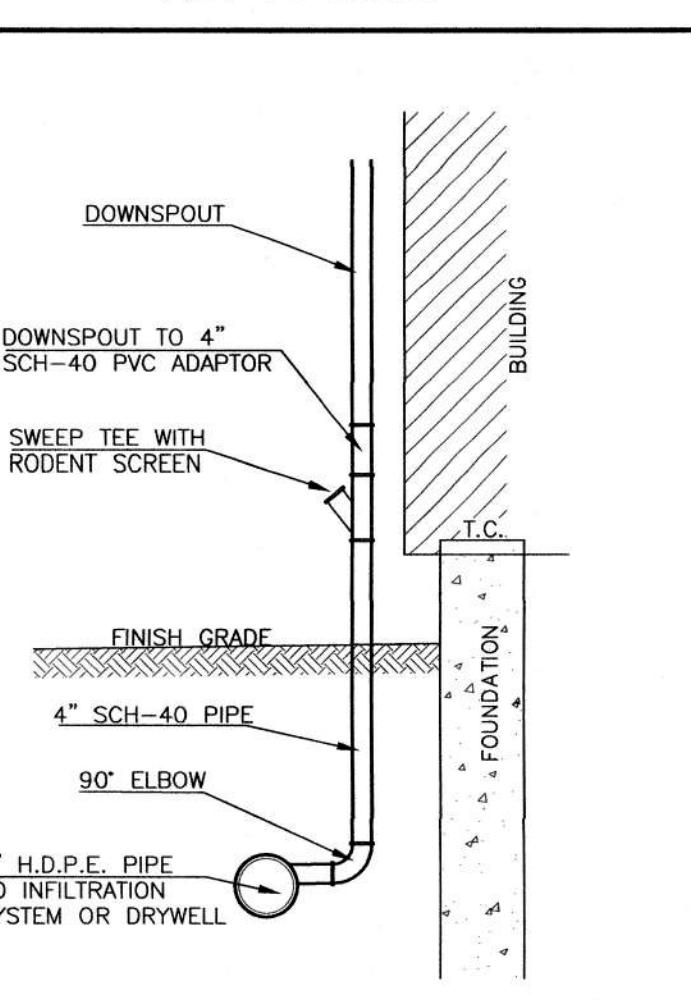
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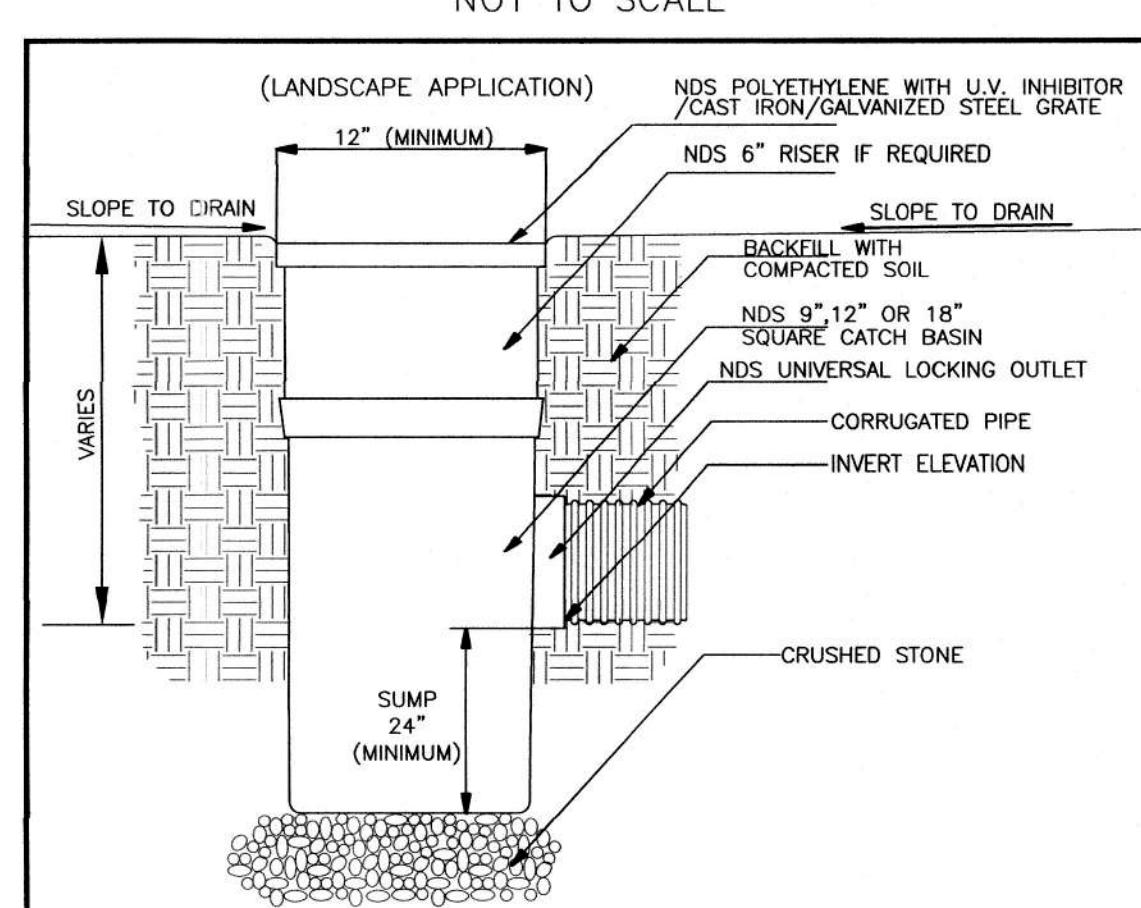
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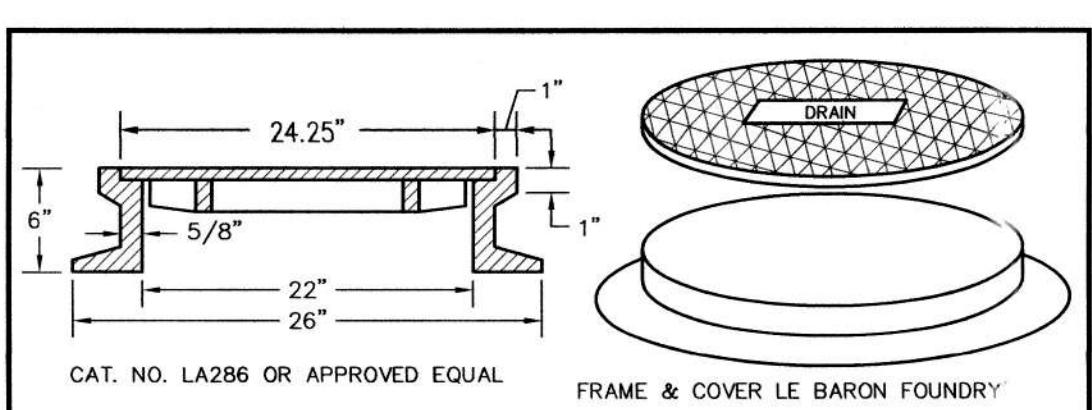
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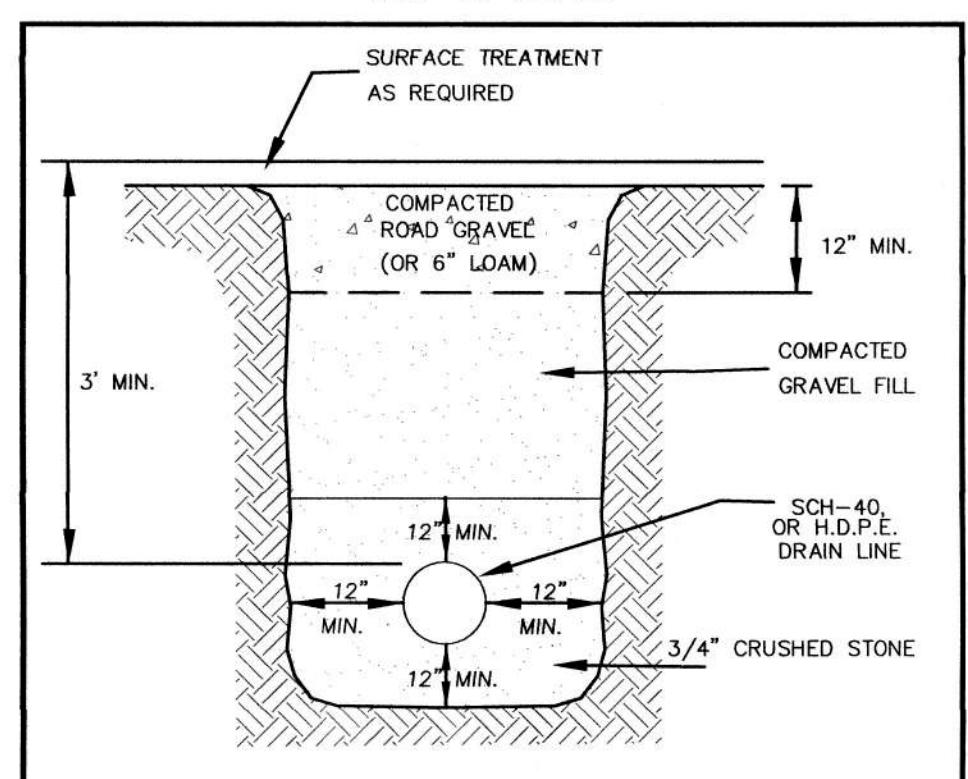
NDS AREA DRAIN
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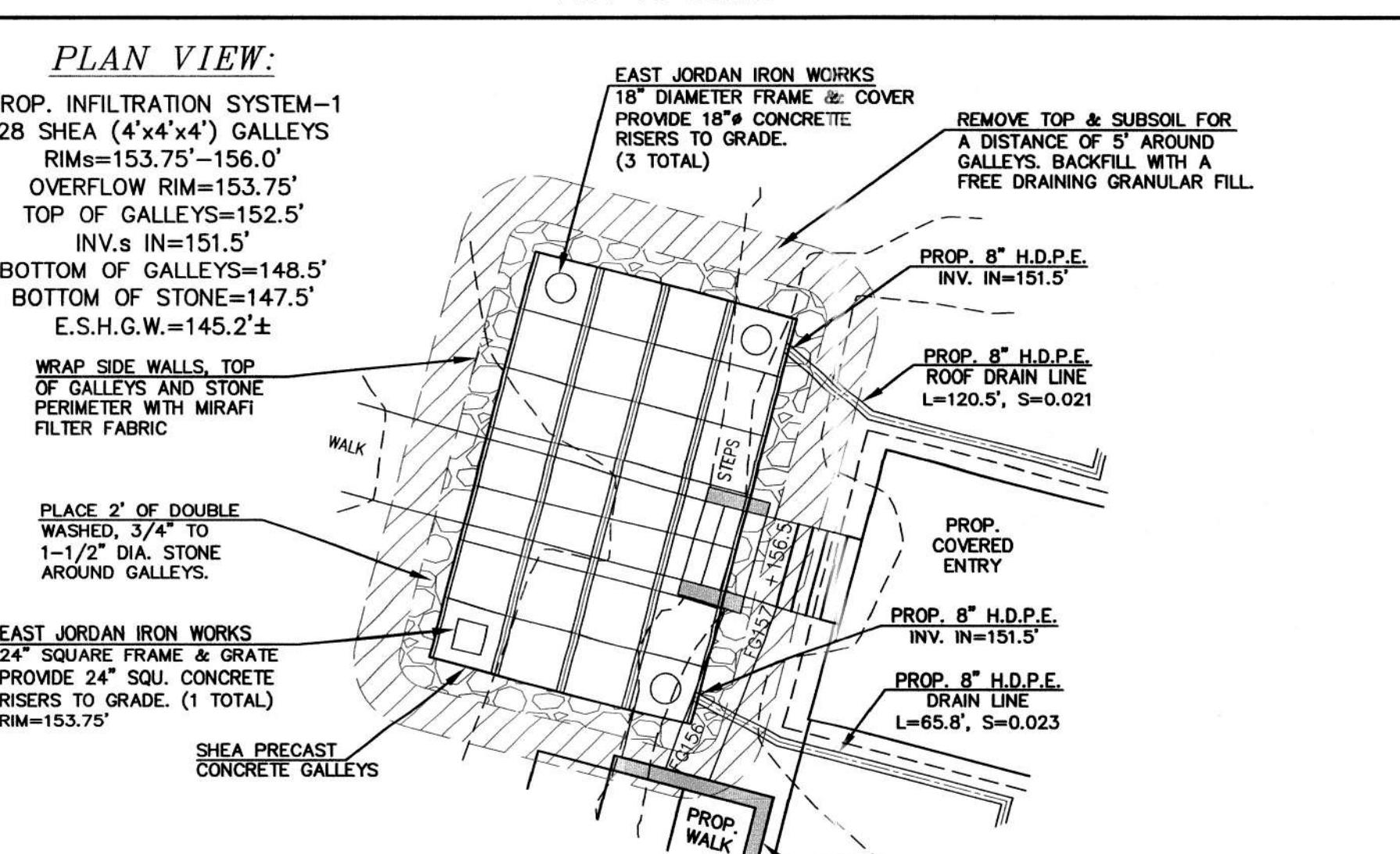
STANDARD DRAIN MANHOLE FRAME & COVER
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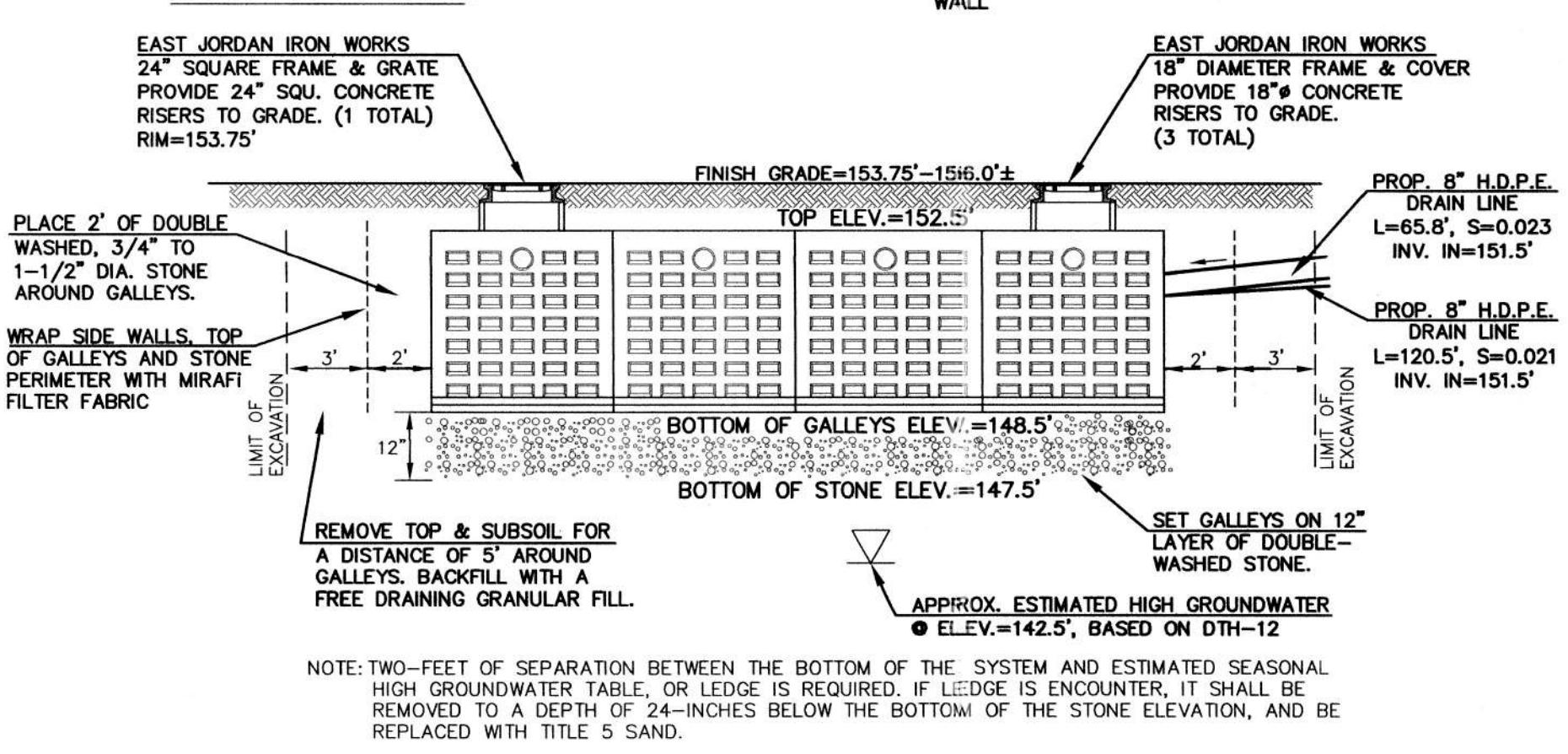
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NOT TO SCALE



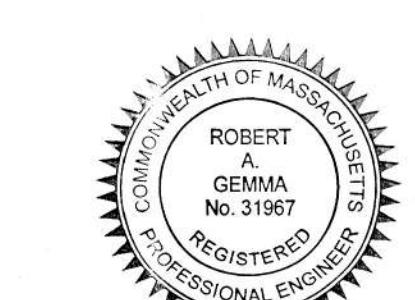
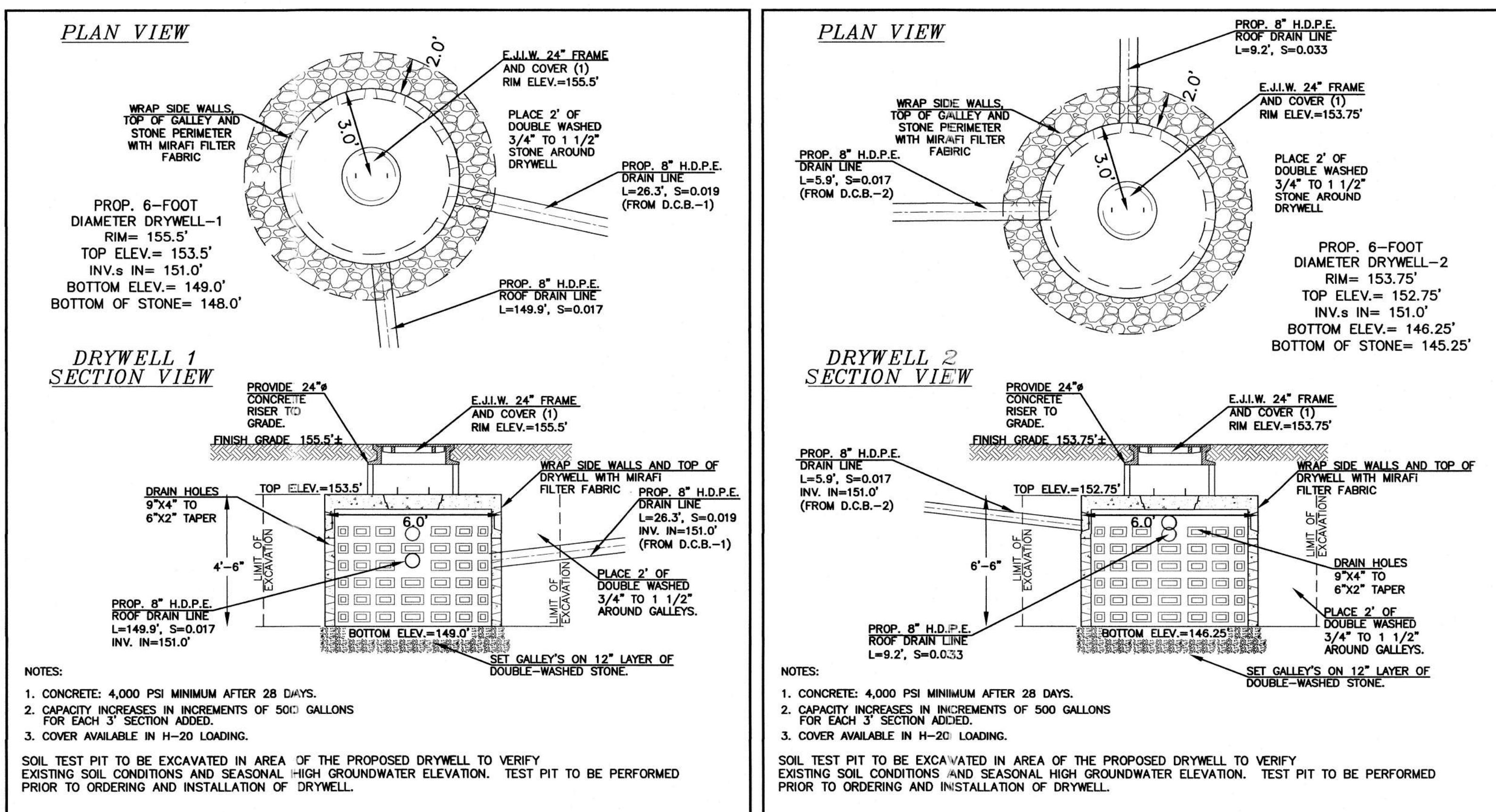
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NOT TO SCALE



PROFILE VIEW:



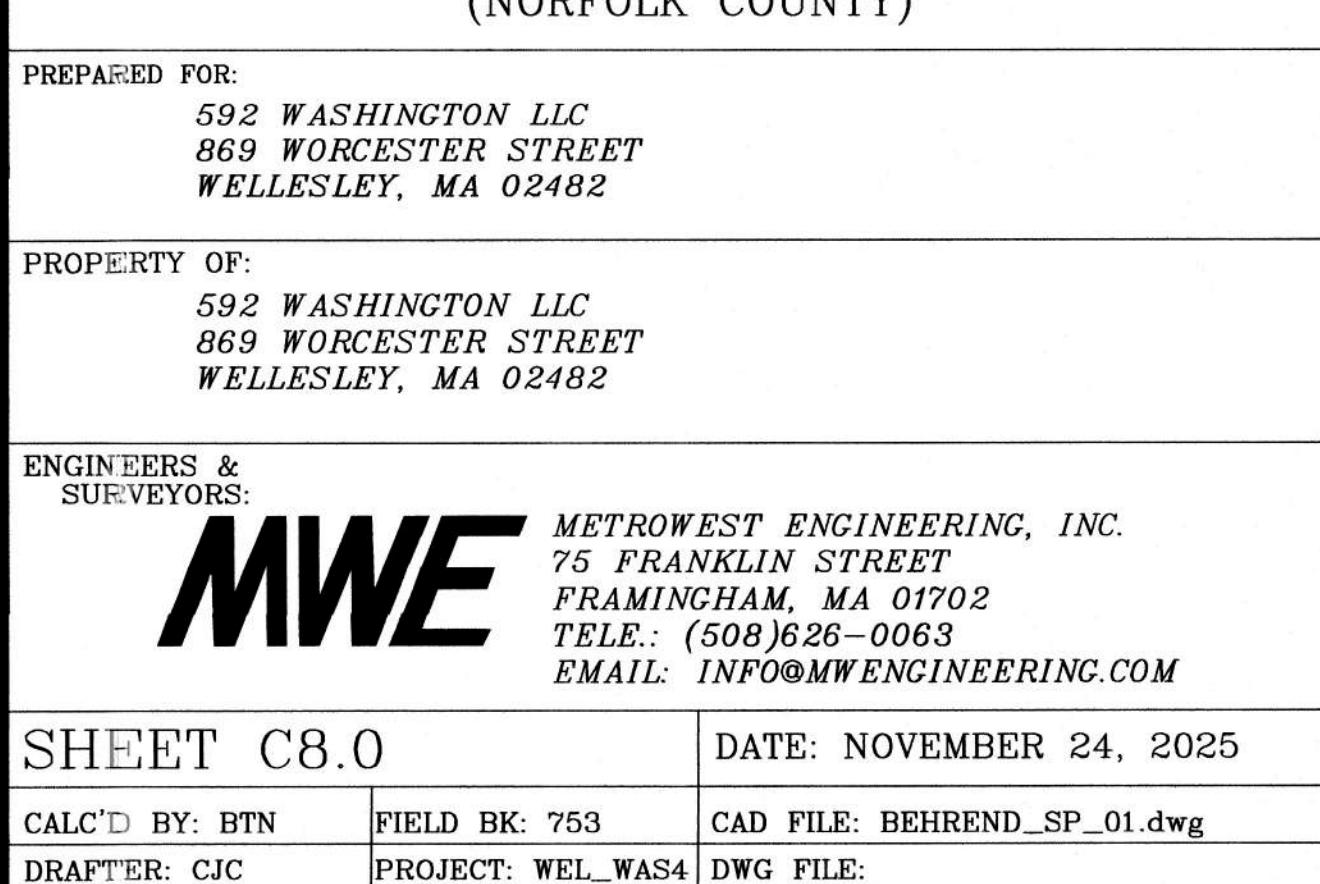
PROPOSED 6-FOOT PRECAST CONCRETE DRYWELL-1 PROPOSED 6-FOOT PRECAST CONCRETE DRYWELL-2
NOT TO SCALE



FOR METROWEST ENGINEERING, INC. DATE
ROBERT A. GEMMA, P.E. # 31967 (CIVIL)

GRAPHIC SCALE
1 inch = 20 ft.
(FEET)
0 4 8 12 16 20 24 28 32
(METERS) 0 2 4 6 8 10 12 14 16 18 20 22 24 26 28 30

DETAILS PLAN
#592 WASHINGTON STREET
IN
WELLESLEY, MASS
(NORFOLK COUNTY)



SHEET C8.0

DATE: NOVEMBER 24, 2025

CALC'D BY: BTN

FIELD BK: 753

CAD FILE: BEHREND_SP_01.dwg

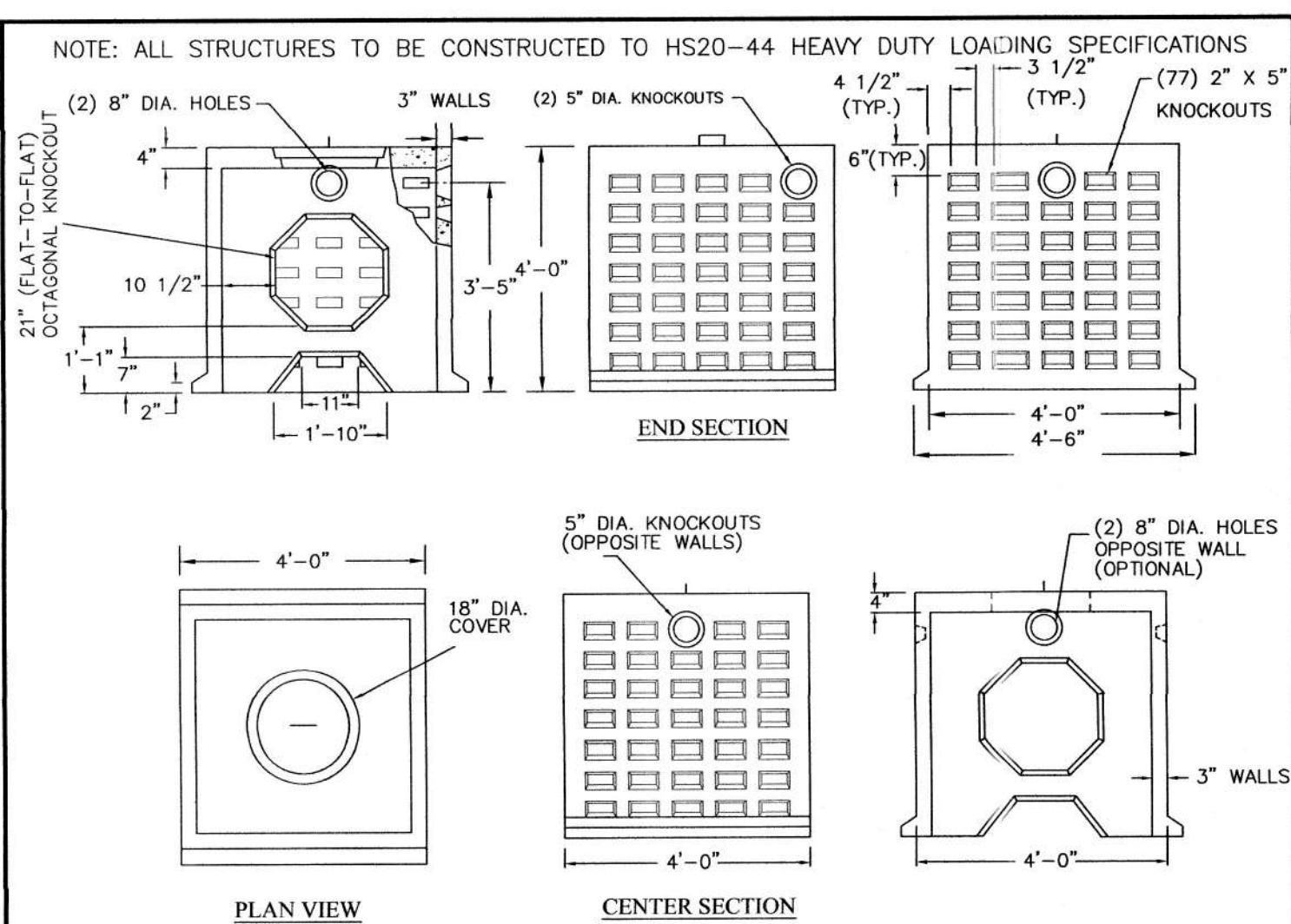
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PROJECT: WEL_WAS4

DWG FILE:

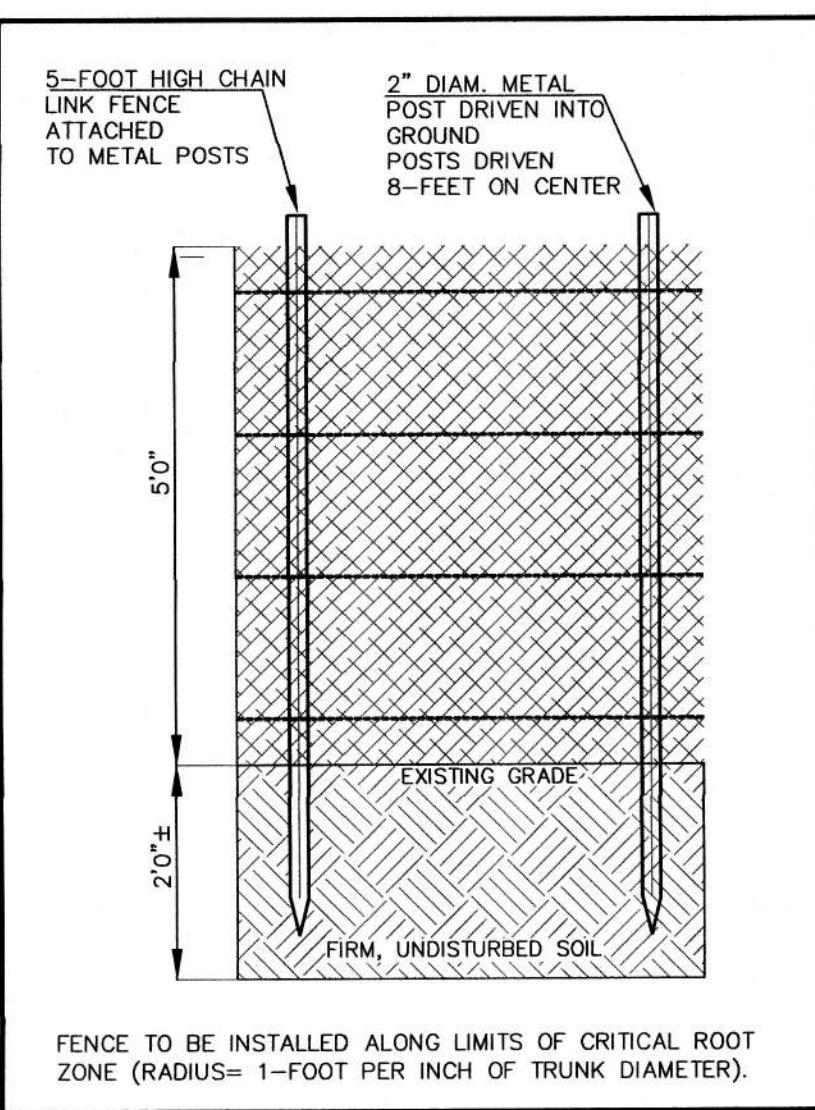
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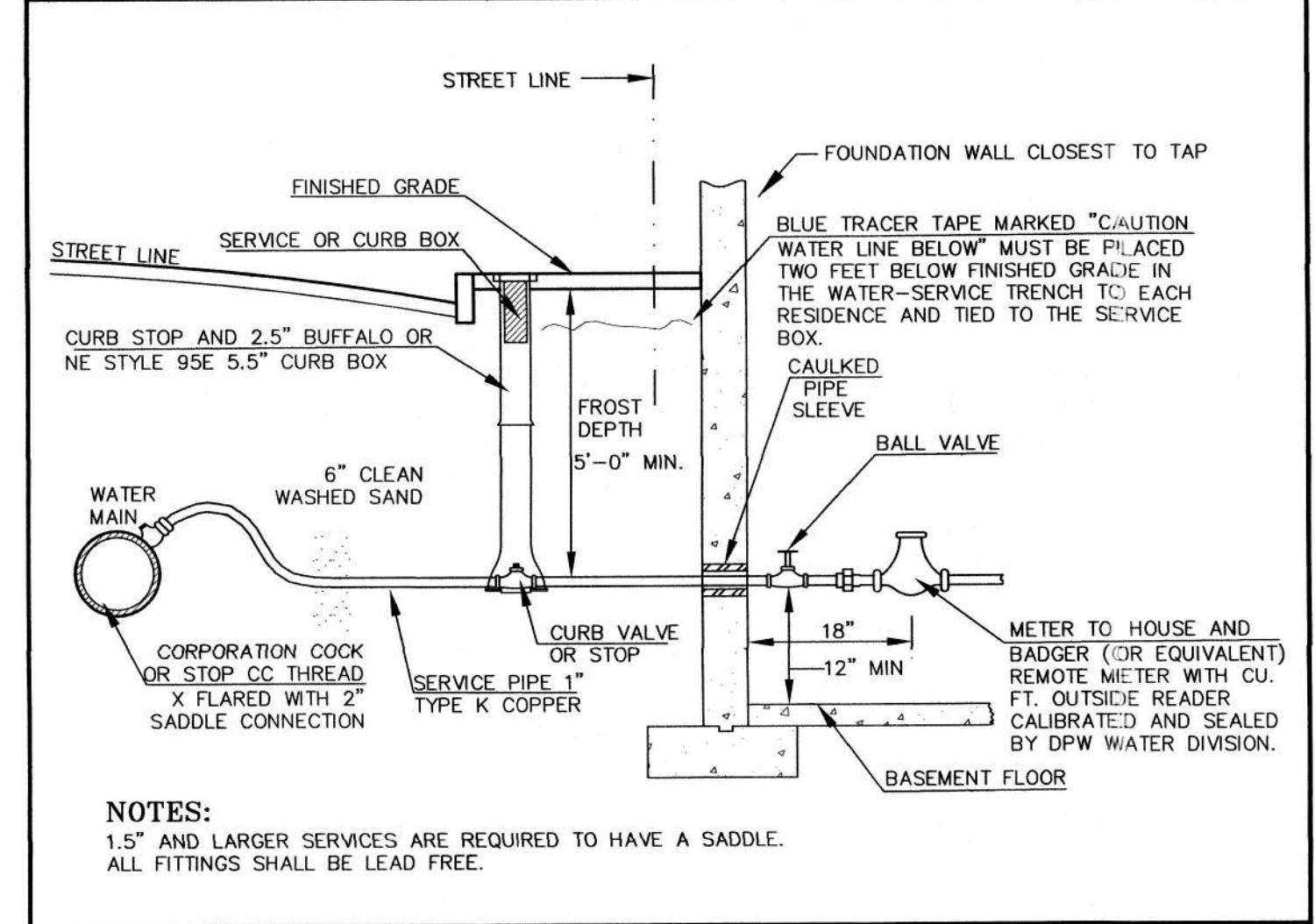
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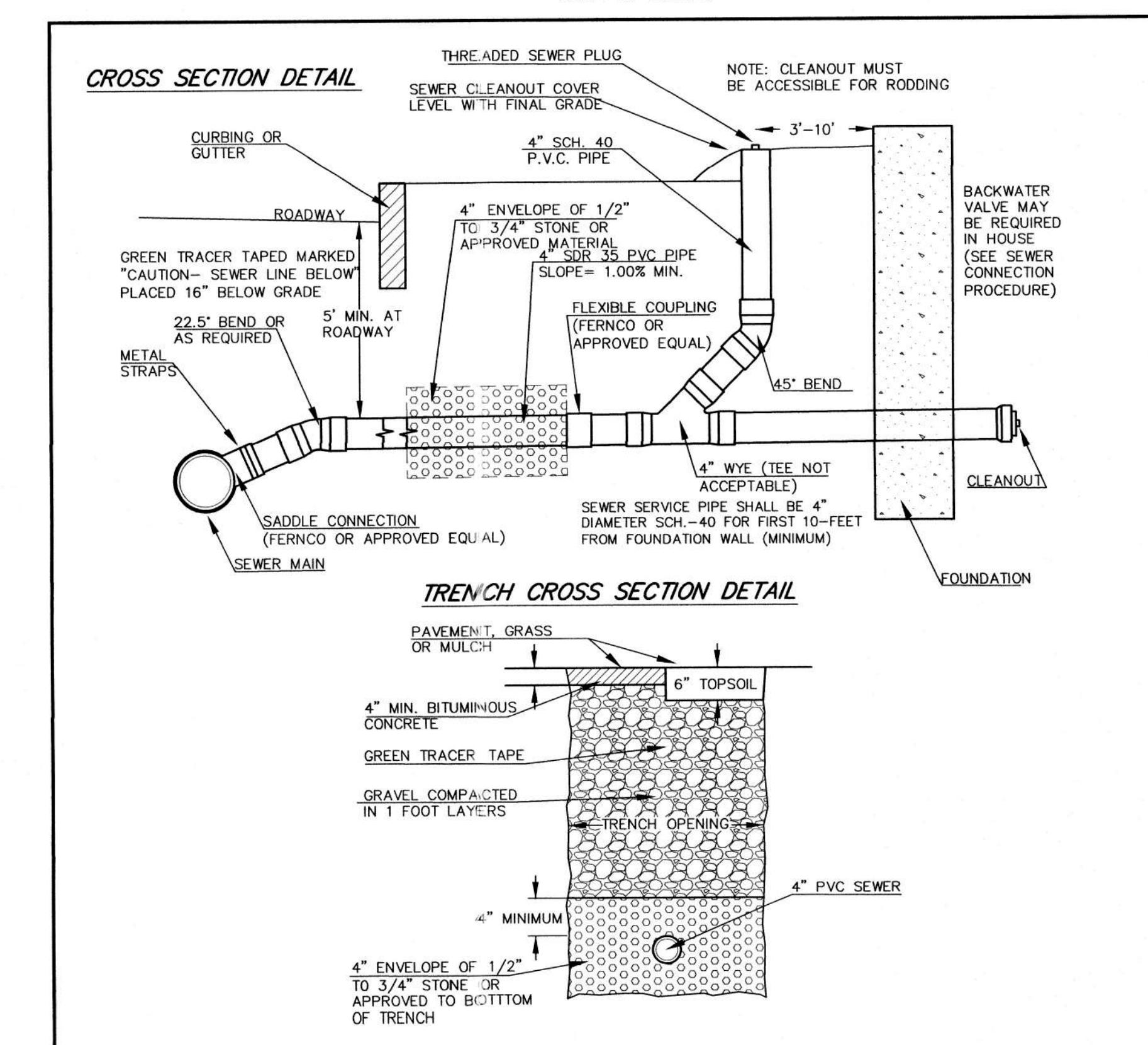
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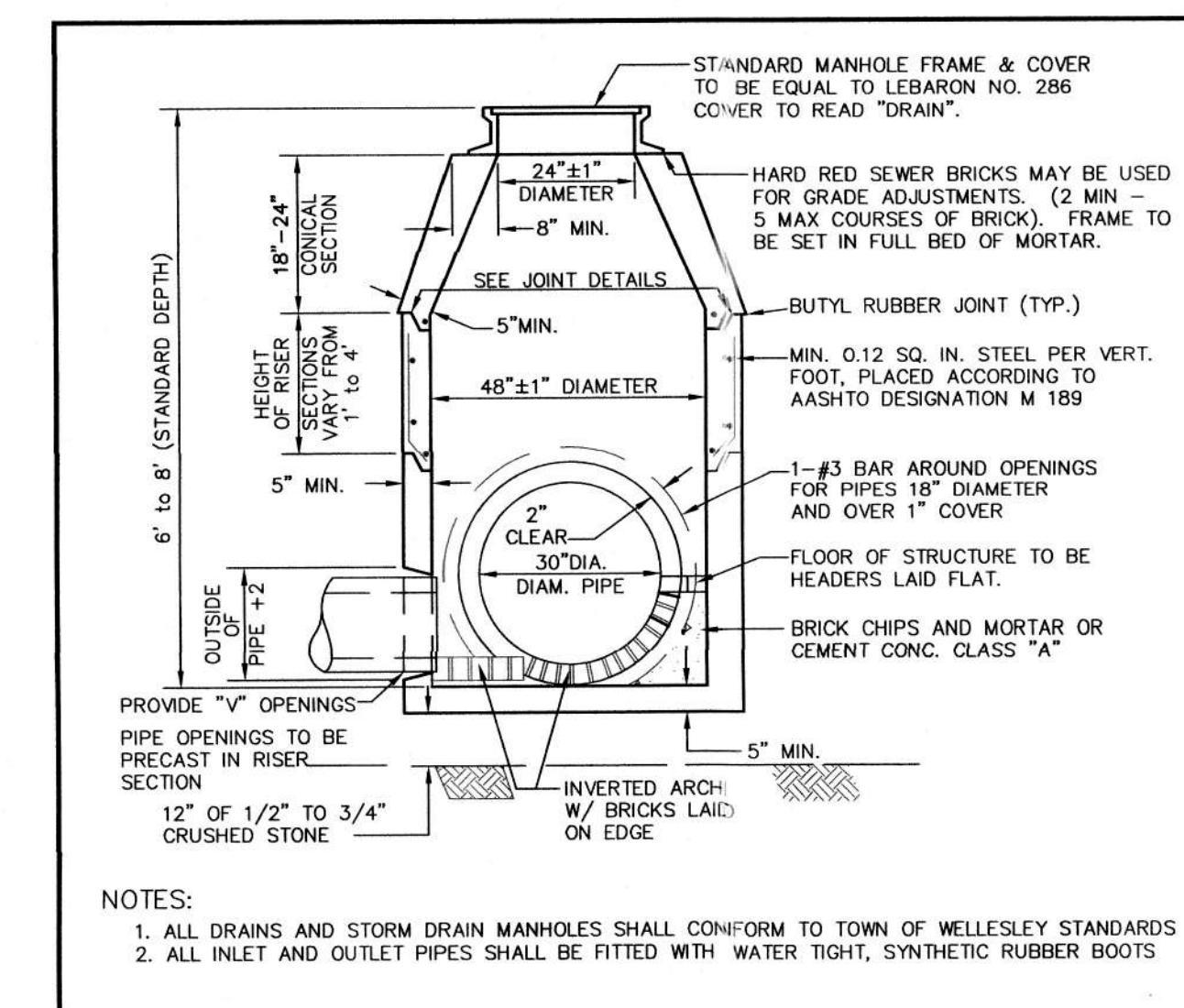
TYPICAL SEWER SERVICE CONNECTION WITH OUTSIDE CLEANOUT

NOT TO SCALE



PRECAST CONCRETE DRAIN MANHOLE

NOT TO SCALE



FOR METROWEST ENGINEERING, INC. DATE
ROBERT A. GEMMA, P.E. # 31967 (CIVIL)

GRAPHIC SCALE
1 inch = 20 ft.

20 0 10 20 40 (FEET) 80
0 4 10 20 20 30 (METERS)

DETAILS PLAN #592 WASHINGTON STREET IN WELLESLEY, MASS (NORFOLK COUNTY)

PREPARED FOR:
592 WASHINGTON LLC
869 WORCESTER STREET
WELLESLEY, MA 02482

PROPERTY OF:
592 WASHINGTON LLC
869 WORCESTER STREET
WELLESLEY, MA 02482

ENGINEERS & SURVEYORS:
MWE METROWEST ENGINEERING, INC.
75 FRANKLIN STREET
FRAMINGHAM, MA 01702
TELE: (508)626-0063
EMAIL: INFO@MWEENGINEERING.COM

SHEET C8.1

DATE: NOVEMBER 24, 2025

CALC'D BY: BTN

FIELD BK: 753

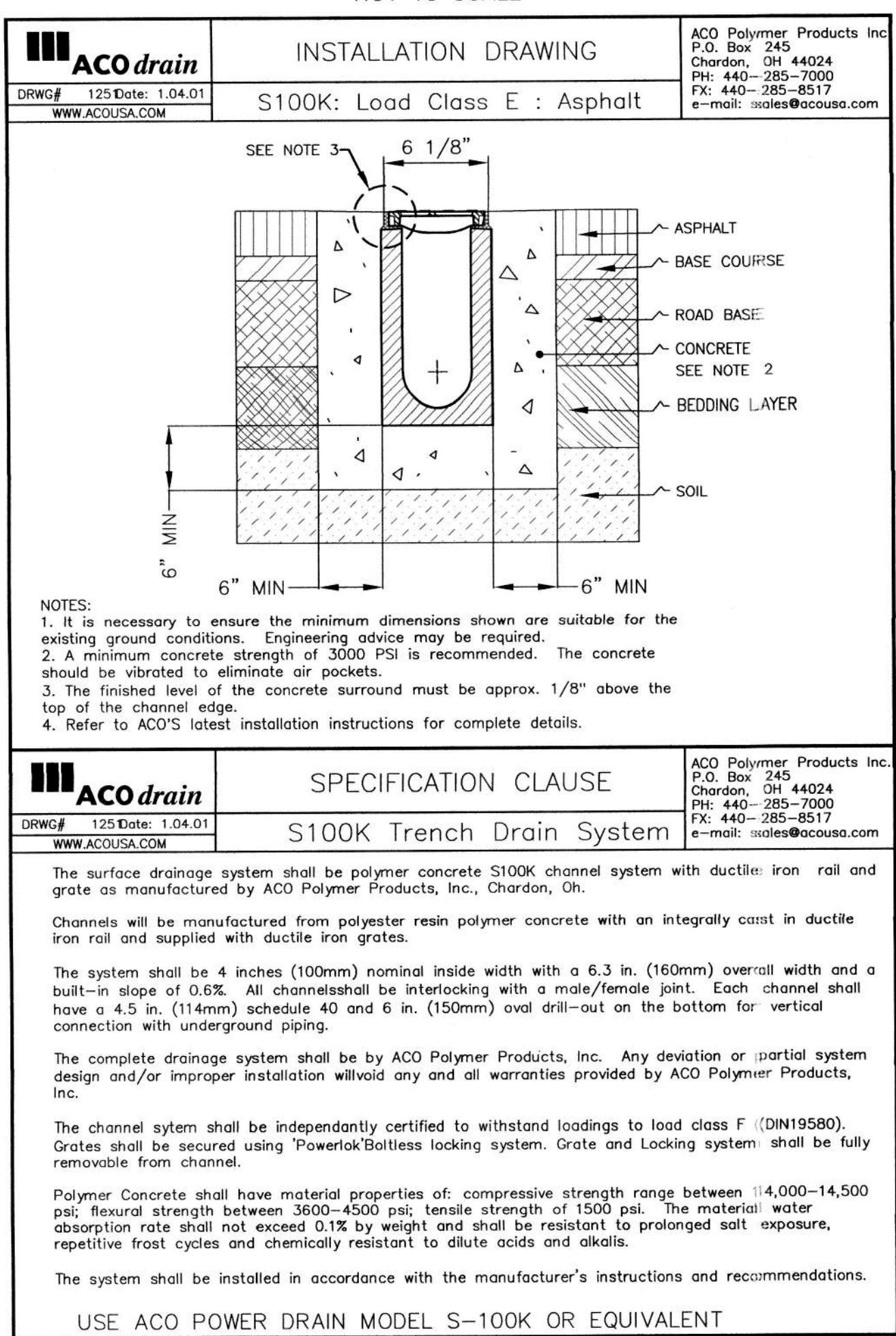
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PROJECT: WEL_WAS4

DWG FILE: SP112425

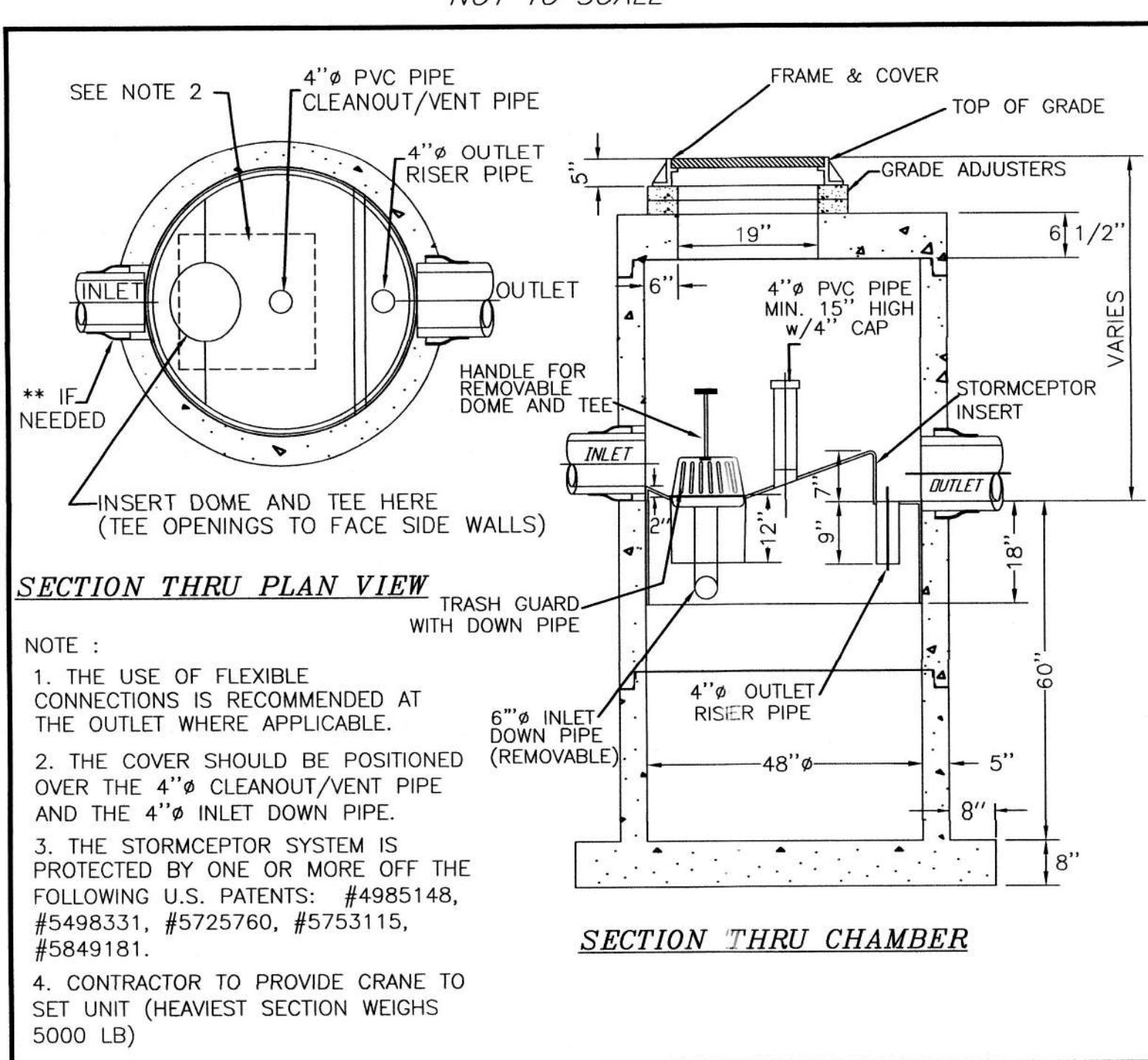
TRENCH DRAIN

NOT TO SCALE



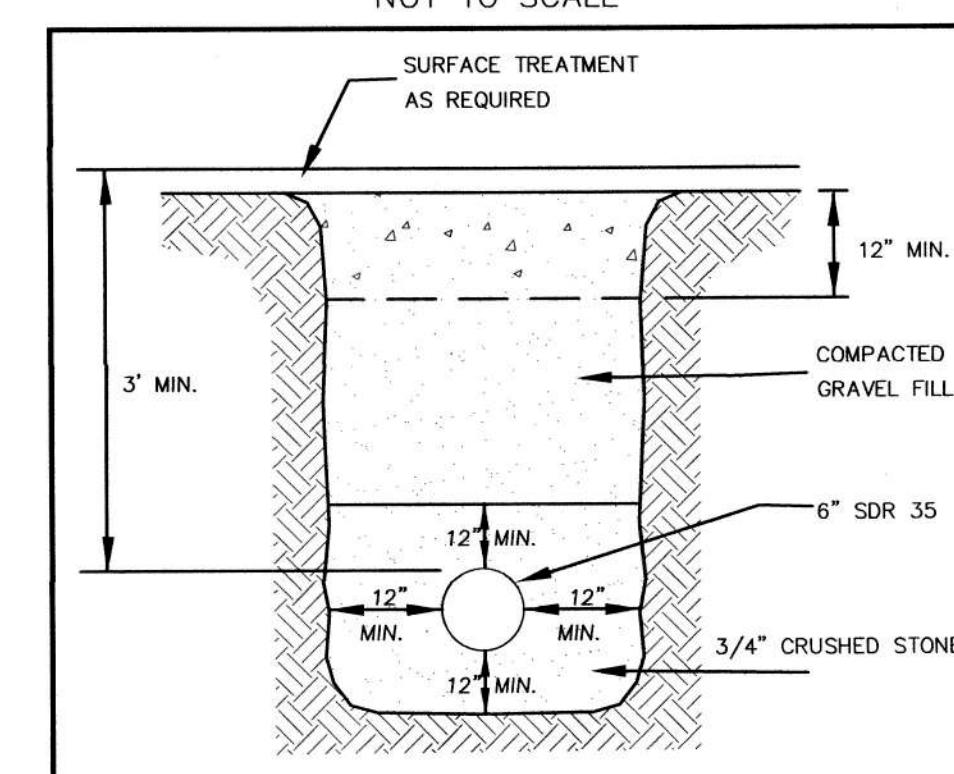
STORMCEPTOR (STC-450)

NOT TO SCALE



TYPICAL SEWER TRENCH

NOT TO SCALE





















Proposed
Multi Family
Use

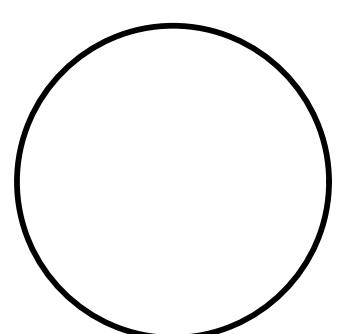
592
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Renderings

JOB NO. **T-1.1**
DATE 09.19.2025
DWG BY RJM
CKD BY MLM
SCALE 1" = 10'

PROPOSED RENOVATION AND ADDITION TO:

**592 WASHINGTON STREET
WELLESLEY, MA**

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Renderings

JOB NO

DATE 09.19.2025

DWG BY RJM

CKD BY MLM

SCALE 1' = 10'

T-1.2

Proposed
Multi Family
Use

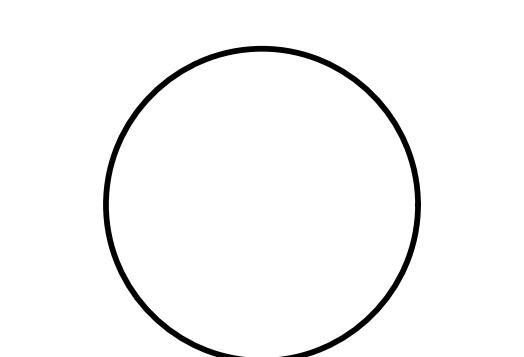
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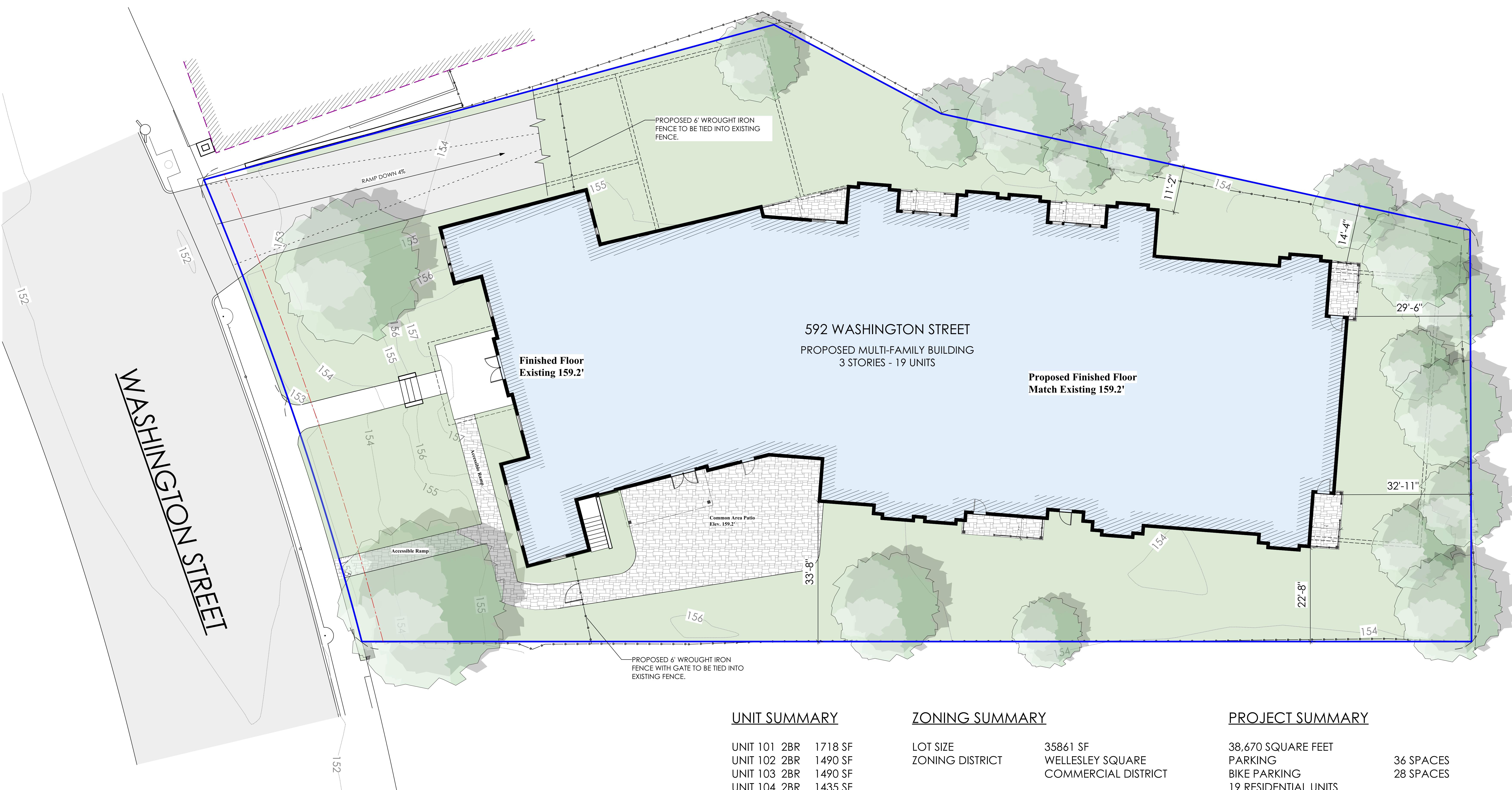


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Architectural
Site Plan

JOB NO	L-1.1
DATE	09.19.2025
DWG BY	RJM
CKD BY	MLM
SCALE	1" = 10'



UNIT SUMMARY

UNIT 101	2BR	1718 SF
UNIT 102	2BR	1490 SF
UNIT 103	2BR	1490 SF
UNIT 104	2BR	1435 SF
UNIT 105	3BR	1887 SF
UNIT 106	3BR	1704 SF
UNIT 201	2BR	1718 SF
UNIT 202	2BR	1490 SF
UNIT 203	2BR	1490 SF
UNIT 204	2BR	1435 SF
UNIT 205	3BR	1887 SF
UNIT 206	3BR	1704 SF
UNIT 207	3BR	2277 SF
UNIT 208	2BR	1926 SF
UNIT 301	2BR	1473 SF
UNIT 302	1BR	977 SF
UNIT 303	2BR	1365 SF
UNIT 304	2BR	1376 SF
UNIT 305	3BR	1687 SF

ZONING SUMMARY

LOT SIZE	35861 SF
ZONING DISTRICT	WELLESLEY SQUARE COMMERCIAL DISTRICT
MINIMUM AREA:	NA
MINIMUM FRONTAGE:	NA
FRONT SETBACK:	5 FEET
SIDE SETBACK:	NA
REAR SETBACK:	NA
MAXIMUM HEIGHT:	45 FEET / 3 STORIES
PROPOSED HEIGHT:	40 FEET / 3 STORIES
ACCESSORY NON RESIDENTIAL:	RESIDENTIAL FLOOR AREA: 5,650 SF
PARKING:	19 SPACES 36 SPACES
4 INCLUSIONARY UNITS PROVIDED (21%)	

PROJECT SUMMARY

38,670 SQUARE FEET	
PARKING	
BIKE PARKING	
19 RESIDENTIAL UNITS	
01 ONE BEDROOM	
12 TWO BEDROOM	
06 THREE BEDROOM	
36 SPACES	
28 SPACES	
TOTAL PARKING SPACES	-36
STRUCTURED BIKE PARKING	-14

Parking Summary

OFF-STREET CAR PARKING	-2
ACCESSIBLE SPACES	-34

RESIDENCE SPACES	
	-36

STRUCTURED BIKE PARKING	-14
-------------------------	-----

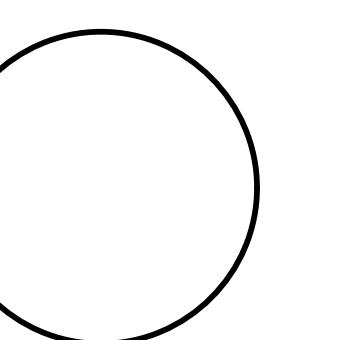
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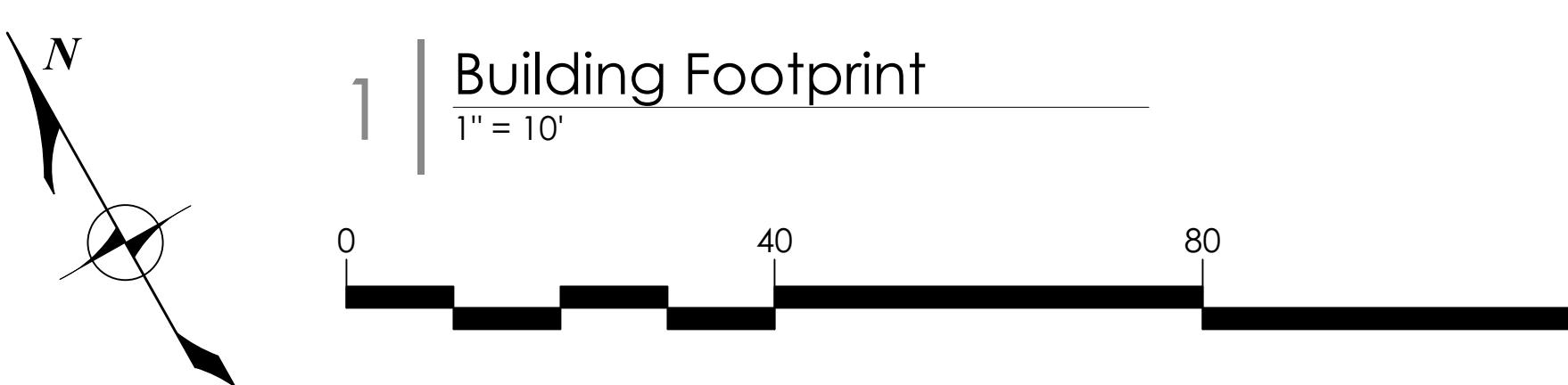
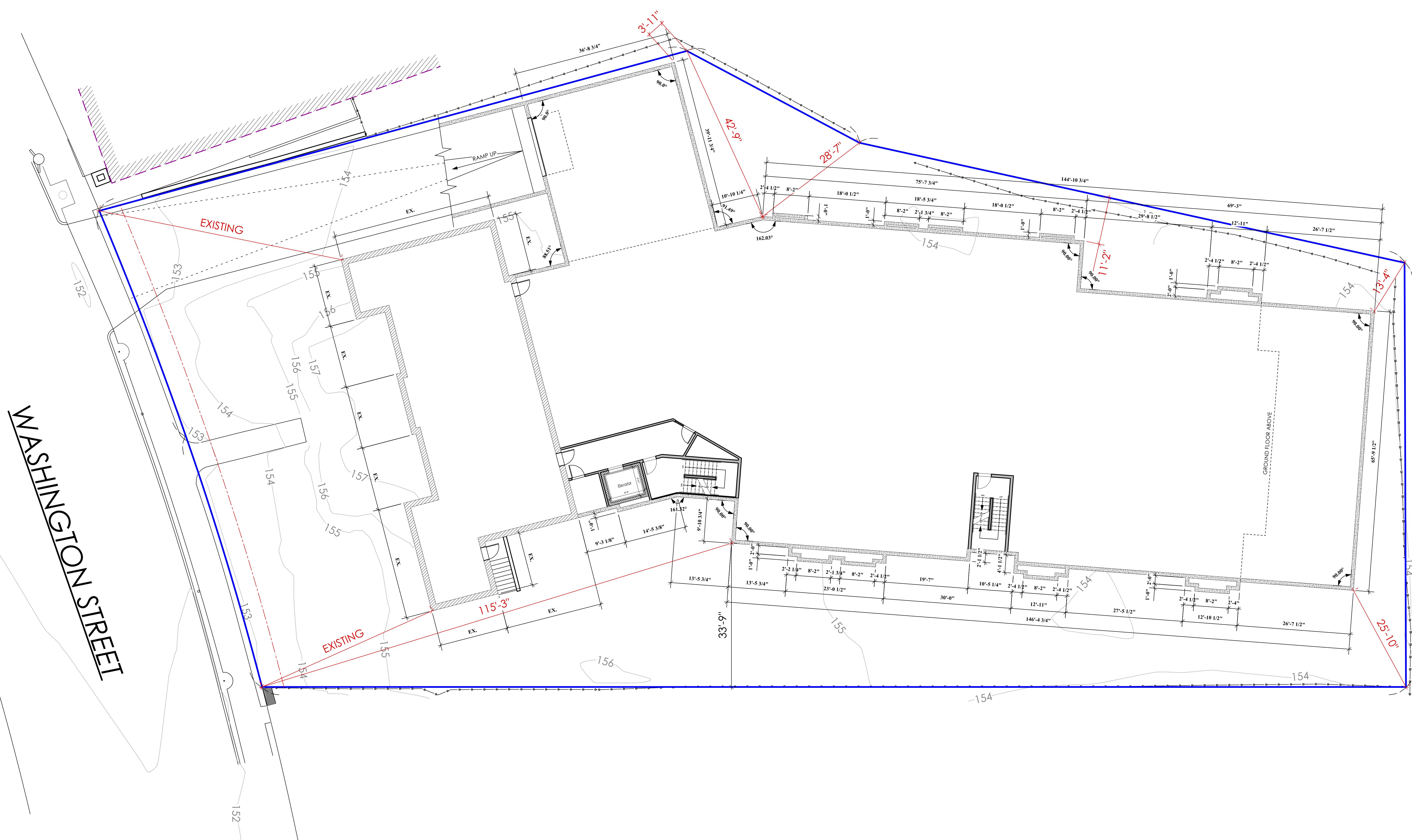


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Building
Footprint

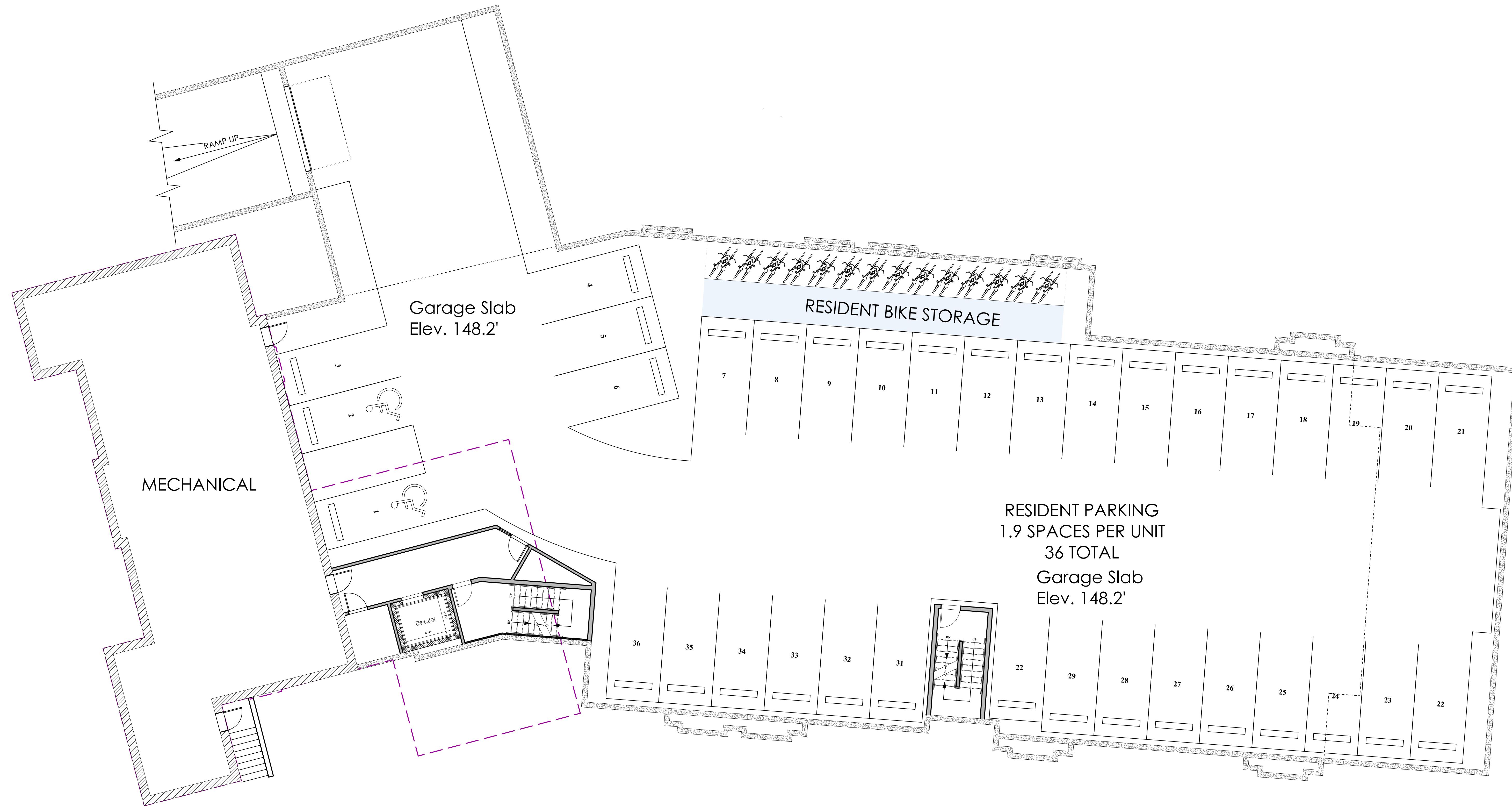
JOB NO	DATE	09.19.2025
DWG BY	RJM	
CKD BY	MLM	
SCALE		1" = 10'



Proposed Multi Family Use

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1 | Garage Floor Plan

A-1.1 1/8" = 1'-0"

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<h1>Garage Floor Plan</h1>	
JOB NO	
DATE 09.19.2025	
WG BY	RJM
KD BY	MLM
SCALE	
1/8" = 1'-0"	
<h1>A-1.1</h1>	

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1 | Ground Floor Plan - 14,475 SF

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Ground Floor Plan

JOB NO

Page 10

ATE 09.1

CC BY

CALE

1/8"

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Proposed
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Use

592
Washington St.
Wellesley, MA

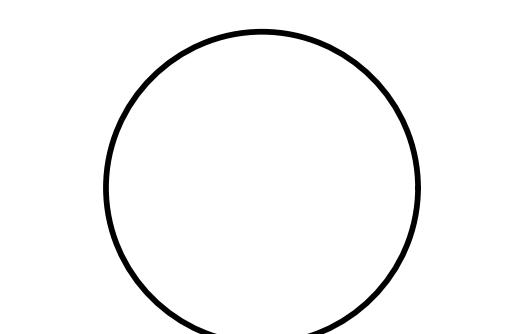
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1 | Third Floor Plan - 10,360 SF
A-1.4 1/8" = 1'-0"

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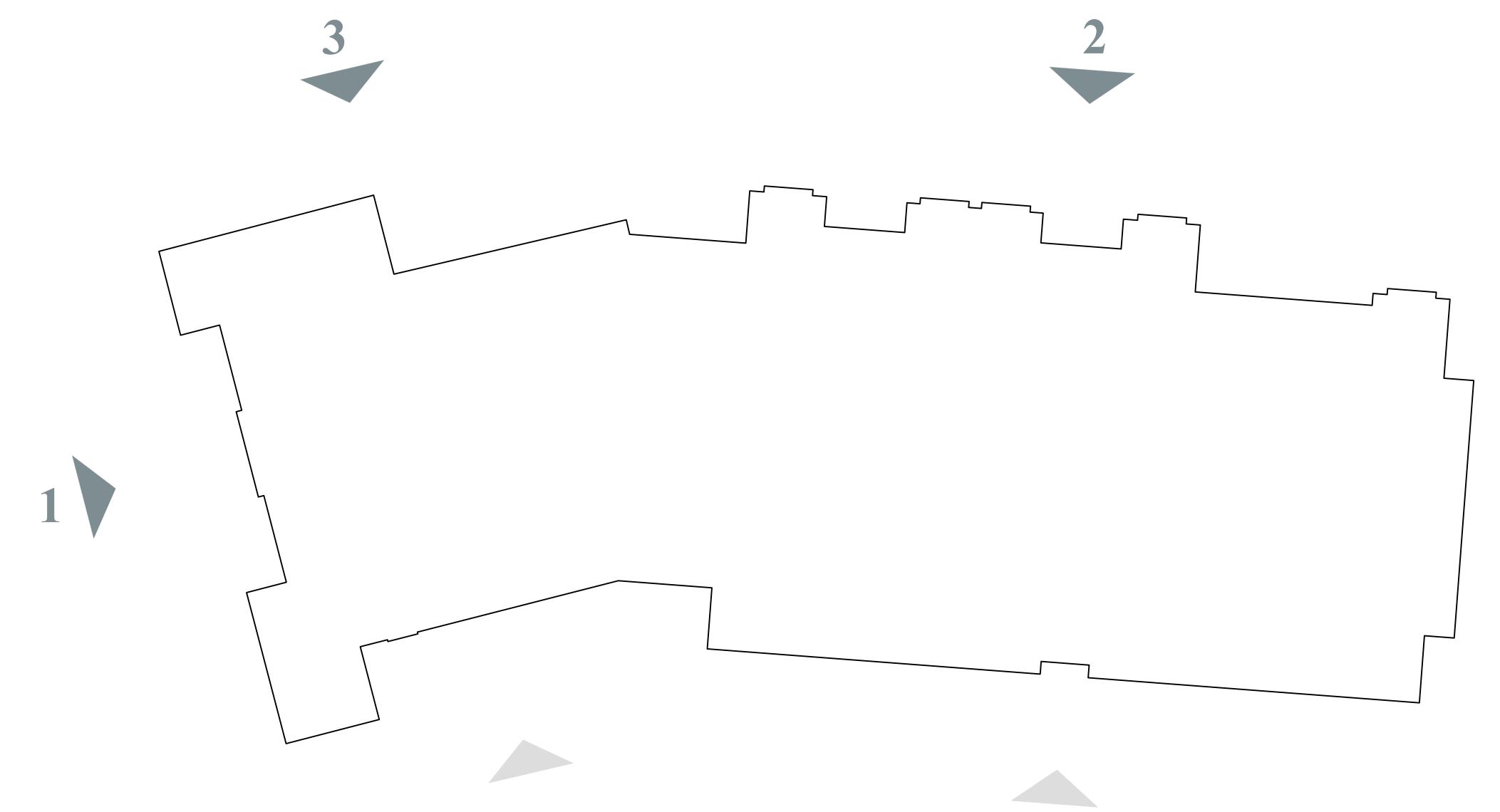
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Third Floor Plan	A-1.4
JOB NO	
DATE	09.19.2025
DWG BY	RJM
CKD BY	MLM
SCALE	1/8" = 1'-0"

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Elevations

JOB NO

DATE

DWG BY

CKD BY

SCALE

09.19.2025

RJM

MLM

1/8" = 1'-0"

A-2.1

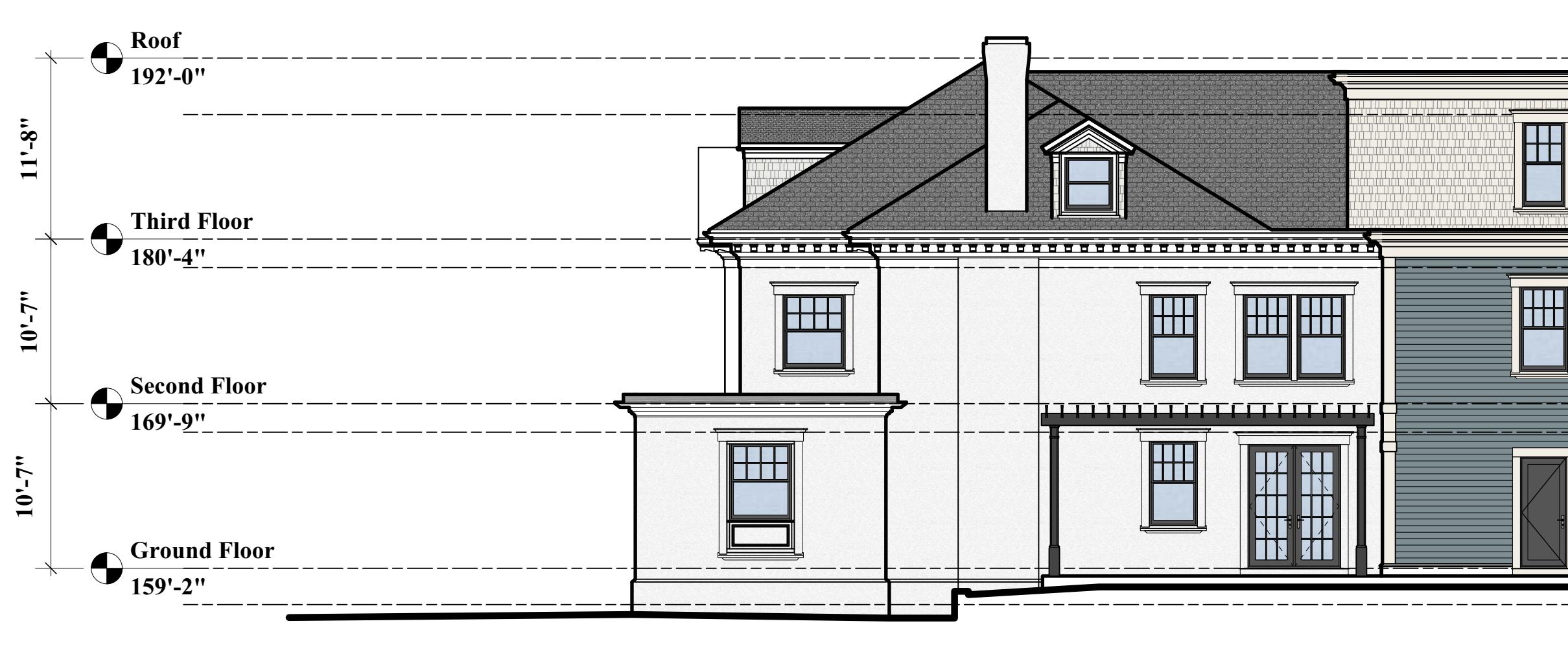
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Use

592
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1 | South-West Facing Elevation
A-2.2 | 1/8" = 1'-0"



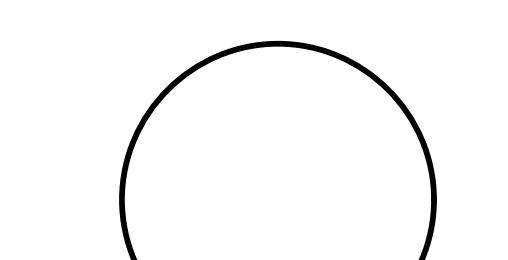
2 | South Facing Elevation
A-2.2 | 1/8" = 1'-0"



3 | South-East Facing Elevation
A-2.2 | 1/8" = 1'-0"

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Elevations

JOB NO
DATE 09.19.2025
DWG BY RJM
CKD BY MLM
SCALE 1/8" = 1'-0"

A-2.2

Transportation Impact Assessment

Proposed Multifamily Residential Development
592 Washington Street
Wellesley, Massachusetts

Prepared for:

592 Washington Street LLC
Wellesley Hills, Massachusetts

November 2025

Prepared by:



35 New England Business Center Drive
Suite 140
Andover, MA 01810



35 New England Business Center Drive
Suite 140
Andover, MA 01810

Dear Reviewer:

This letter shall certify that this *Transportation Impact Assessment* has been prepared under my direct supervision and responsible charge. I am a Registered Professional Engineer (P.E.) in the Commonwealth of Massachusetts (Massachusetts P.E. No. 38871, Civil) and hold Certification as a Professional Traffic Operations Engineer (PTOE) from the Transportation Professional Certification Board, Inc. (TPCB), an independent affiliate of the Institute of Transportation Engineers (ITE) (PTOE Certificate No. 993). I am also a Fellow of the Institute of Transportation Engineers (FITE).

Sincerely,

VANASSE & ASSOCIATES, INC.

A handwritten signature in black ink that reads "Jeffrey S. Dirk".

Jeffrey S. Dirk, P.E., PTOE, FITE
Managing Partner

CONTENTS

EXECUTIVE SUMMARY	5
INTRODUCTION	9
Project Description	9
Study Methodology	10
EXISTING CONDITIONS	11
Roadway	11
Intersections	12
Traffic Volumes	12
Pedestrian and Bicycle Facilities	14
Public Transportation	14
Spot Speed Measurements	15
Motor Vehicle Crash Data	15
FUTURE CONDITIONS	18
Future Traffic Growth	18
Project-Generated Traffic	19
Trip Distribution and Assignment	20
Future Traffic Volumes – Build Condition	20
TRAFFIC OPERATIONS ANALYSIS	22
Methodology	22
Analysis Results	25

CONTENTS (Continued)

SIGHT DISTANCE EVALUATION.....	29
CONCLUSIONS AND RECOMMENDATIONS	31
Conclusions	31
Recommendations	32

FIGURES

No.	Title
1	Site Location Map
2	Existing Intersection Lane Use, Travel Lane Width and Pedestrian Facilities
3	2025 Existing Weekday Peak-Hour Traffic Volumes
4	2032 No Build Peak-Hour Traffic Volumes
5	Trip Distribution Map
6	Project Generated Peak-Hour Traffic Volumes
7	2032 Build Peak-Hour Traffic Volumes

TABLES

No.	Title
1	Study Area Intersection Description
2	2025 Existing Traffic Volumes
3	Vehicle Travel Speed Measurements
4	Motor Vehicle Crash Data Summary
5	Trip-Generation Summary
6	Peak-Hour Traffic-Volume Increases
7	Level-of-Service Criteria for Signalized Intersections
8	Level-of-Service Criteria for Unsignalized Intersections
9	Signalized Intersection Level-of-Service and Vehicle Queue Summary
10	Unsignalized Intersection Level-of-Service and Vehicle Queue Summary
11	Sight Distance Measurements

EXECUTIVE SUMMARY

Vanasse & Associates, Inc. (VAI) has conducted a Transportation Impact Assessment (TIA) in order to determine the potential impacts on the transportation infrastructure associated with the proposed construction of a multifamily residential development to be located at 592 Washington Street (Route 16) in Wellesley, Massachusetts (hereafter referred to as the “Project”). This assessment was prepared in consultation with the Massachusetts Department of Transportation (MassDOT) and the Town of Wellesley; was performed in accordance with MassDOT’s *Transportation Impact Assessment (TIA) Guidelines* and the Traffic Review standards for a Project of Significant Impact (PSI) as defined in Section 5.6 C 3 of the Town of Wellesley Zoning Bylaw; and was conducted pursuant to the standards of the Traffic Engineering and Transportation Planning professions for the preparation of such reports.

Based on this assessment, we have concluded the following with respect to the Project:

1. Using trip-generation statistics published by the Institute of Transportation Engineers (ITE)¹ and without adjustment to account for the use of alternative modes of transportation to Single-Occupant Vehicles (SOVs), the Project is expected to generate approximately 228 vehicle trips on an average weekday, with 20 vehicle trips expected during the weekday morning peak-hour and 16 vehicle trips expected during the weekday evening peak hour;
2. The Project will not result in a significant impact (increase) on motorist delays or vehicle queuing over Existing or anticipated future conditions without the Project (No-Build conditions), with no (0) changes in level of service (LOS) shown to occur as a result of the addition of Project-related traffic and Project-related impacts generally defined as an increase in average motorist delay that resulted in a corresponding increase in vehicle queuing of up to one (1) vehicle;
3. Motorists exiting the Project site driveway to Washington Street were shown to operate at LOS B during both peak hours with negligible vehicle queuing. All movements along Washington Street approaching the driveway are predicted to operate at LOS A, also with negligible vehicle queuing.

¹*Trip Generation*, 12th Edition; Institute of Transportation Engineers; Washington, DC; August 2025.

4. Independent of the Project, the Washington Street/Central Street/Grove Street intersection was identified to have a motor vehicle crash rate that was above the MassDOT District 6 average crash rate but that was below the statewide average crash rate for similar intersections. As such, specific recommendations have been provided to enhance safety at the intersection for consideration independent of the Project (see *Recommendations*); and
5. Lines of sight to and from the Project site driveway intersection with Washington Street were found to exceed the recommended minimum distance for safe operation based on the appropriate approach speed and with consideration of the downtown setting within which the Project is located.

In consideration of the above, we have concluded that the Project can be accommodated within the confines of the existing transportation infrastructure in a safe and efficient manner with implementation of the recommendations that follow.

RECOMMENDATIONS

A detailed transportation improvement program has been developed that is designed to provide safe and efficient access to the Project site and address any deficiencies identified at off-site locations evaluated in conjunction with this study. The following improvements have been recommended as a part of this evaluation and, where applicable, will be completed in conjunction with the Project subject to receipt of all necessary rights, permits, and approvals.

Project Access

Access to the Project will continue to be provided by way of the existing full access driveway that intersects the east side of Washington Street approximately 120 feet south of Church Street. The following recommendations are offered with respect to the design and operation of the Project site access and internal circulation, many of which are reflected on the Site Plans.

- The Project site driveway will be a minimum of 24-feet in width and designed to accommodate the turning and maneuvering requirements of the largest anticipated responding emergency vehicle.
- Where perpendicular parking is proposed, the drive aisle behind the parking will be a minimum of 23 feet in order to facilitate parking maneuvers.
- Vehicles exiting the Project site to Washington Street will be placed under STOP-sign control with a marked STOP-line provided.
- All signs and pavement markings to be installed within the Project site will conform to the applicable standards of the *Manual on Uniform Traffic Control Devices* (MUTCD).²
- A sidewalk has been provided within the Project site that extends to the existing sidewalk along Washington Street. Crosswalks are provided to the north of the Project site at the Washington Street/Church Street intersection for crossing Washington Street and Church Street.

²*Manual on Uniform Traffic Control Devices (MUTCD)*; Federal Highway Administration; Washington, D.C.; 2009.

- The Project site driveway is and will continue to be a pan-type drive with the sidewalk flush across the driveway. Americans with Disabilities Act (ADA) compliant wheelchair ramps will be provided for any new crosswalks that are constructed as a part of the Project.
- Electric vehicle (EV) charging stations will be installed within the Project site, with a minimum of 20% of the parking spaces to be EV ready.
- Signs, landscaping and other features that are to be installed as a part of the Project within the intersection sight triangle areas will be designed and maintained so as not to restrict lines of sight.
- Snow accumulations (windrows) within the sight triangle areas will be promptly removed where such accumulations would impede sight lines.

Off-Site

Washington Street at Central Street and Grove Street

Independent of the Project, the Washington Street/Central Street/Grove Street intersection was found to have a motor vehicle crash rate that was above the MassDOT District average crash rate but was found to be below the statewide average crash rate. Additionally, overall intersection operations, as well as specific movements from the Washington Street westbound and Grove Street northwestbound approaches are currently operating at or over capacity (i.e., LOS “E” or “F”, respectively) during the peak periods. Independent of the Project, it is recommended that an optimal traffic signal timing and phasing plan be implemented for the intersection to include: i) a review of the “yellow” and “all red” clearance intervals; and ii) consideration of restricting left-turn movements from the Washington Street westbound approach to the Grove Street southeast leg. These left-turn movements are currently permitted across two (2) northbound lanes of traffic from Washington Street which have a protected phase (“green” right-turn arrow display) when motorists are allowed to turn left onto Grove Street.

Transportation Demand Management

Regularly scheduled public transportation services are not currently provided to the Project site. To the north of the Project site, the Massachusetts Bay Transportation Authority (MBTA) provides Commuter Rail service on the Worcester/Framingham Line between Union Station in Worcester and South Station in Boston, with a stop at Wellesley Square Station, which is an approximate 5-minute walking distance of the Project site. In addition, the MWRTA operates the Catch Connect service within the Town of Wellesley, which is an on-demand, curb-to-curb, microtransit service. The service is booked through the MWRTA CATCH app or by phone. The MBTA also operates The Ride paratransit services for eligible persons who cannot use fixed-route transit all or some of the time due to a physical, cognitive, or mental disability in accordance with ADA requirements.

In an effort to encourage the use of alternative modes of transportation to single-occupant vehicles, the following Transportation Demand Management (TDM) measures will be implemented as a part of the Project:

- A transportation coordinator will be assigned for the Project to coordinate the TDM program;

- Information regarding public transportation services, maps, schedules, and fare information will be posted in a central location and/or otherwise made available to residents;
- A “welcome packet” will be provided to new residents detailing available public transportation services, bicycle and walking alternatives, and other commuting options;
- Amenities will be provided to support telecommuting by residents of the Project that may include collaboration space or a business office;
- Pedestrian accommodations have been incorporated within the Project and consist of a walkway that connects to the existing sidewalk along Washington Street;
- A central maildrop and package delivery station will be provided within the building; and
- Secure bicycle parking will be provided for residents that will include weather protected bicycle parking within the parking garage and exterior bicycle parking proximate to the primary building entrance.

With implementation of the aforementioned recommendations, safe and efficient access will continue to be provided to the Project site and the Project can be accommodated within the confines of the existing and improved transportation system.

INTRODUCTION

Vanasse & Associates, Inc. (VAI) has conducted a Transportation Impact Assessment (TIA) in order to determine the potential impacts on the transportation infrastructure associated with the proposed construction of a multifamily residential development to be located at 592 Washington Street (Route 16) in Wellesley, Massachusetts (hereafter referred to as the “Project”). This study evaluates the following specific areas as they relate to the Project: i) access requirements; ii) potential off-site improvements; and iii) safety considerations; and identifies and analyzes existing traffic conditions and future traffic conditions, both with and without the Project, along Washington Street, and at specific intersections located along this roadway through which Project-related traffic will travel.

PROJECT DESCRIPTION

The Project will entail the renovation and expansion of the existing commercial building located at 592 Washington Street (Route 16) in Wellesley, Massachusetts, to accommodate 19 multifamily residential units. The Project site encompasses approximately $0.82\pm$ acres of land that is bounded by residential and commercial properties to the north; residential properties to the south and east; and Washington Street to the west. The Project site is currently improved by two (2) commercial buildings with associated parking areas and appurtenances. The building that fronts along Washington Street will be renovated and expanded to the east and the existing building in the eastern portion of the Project site will be removed to accommodate the Project. Figure 1 depicts the Project site location in relation to the existing roadway network.

Access to the Project will continue to be provided by way of the existing full access driveway that intersects the east side of Washington Street approximately 120 feet south of Church Street.

On-site parking will be provided for 36 vehicles in a parking garage located beneath the residential units, or a parking ratio of 1.89 parking spaces per unit. In addition, weather protected bicycle parking will be provided for 14 bicycles within the parking garage.

The Project site is located within the *Wellesley Square Commercial District*, within which multifamily residential uses are allowed and the parking requirements are defined in Section 5.17, Off-Street Parking, of the Wellesley Zoning Bylaw. For an “apartment building or group of buildings containing three or more dwelling units”, the Zoning Bylaw requires that one (1) parking space per unit be provided, which would require 19 parking spaces for the Project.

Transportation Impact Assessment - Proposed Multifamily Residential Development - Wellesley, Massachusetts

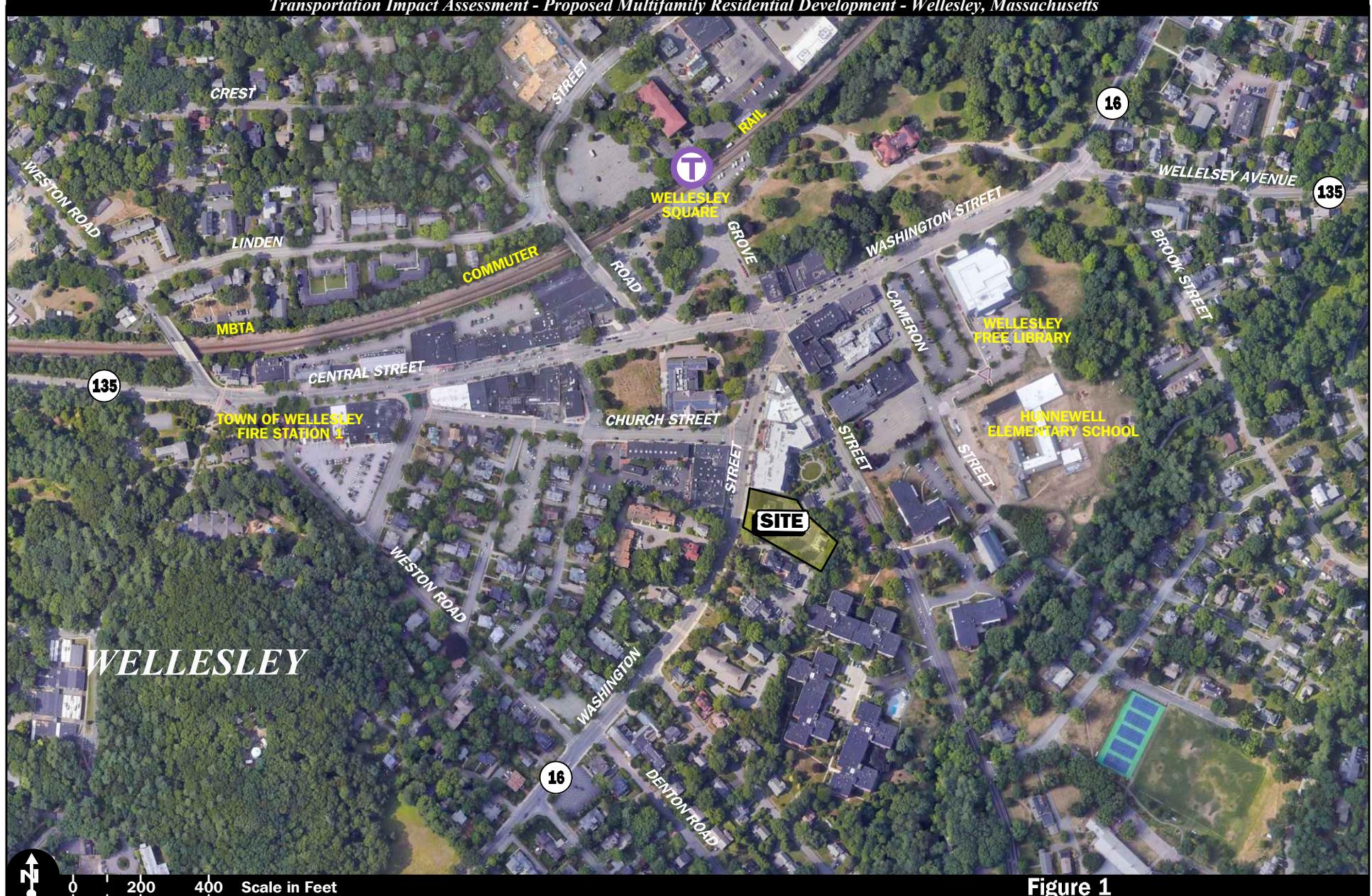


Figure 1

Site Location Map



Given that 36 parking spaces will be provided to support the Project, the parking supply exceeds the requirements of the Zoning Bylaw for the proposed use. In addition, the parking ratio that is proposed (1.89 parking spaces per unit) also exceeds the number of parking spaces that are necessary to accommodate the peak parking demands for a multifamily residential developed in a similar setting as documented by the Institute of Transportation Engineers (ITE).³

STUDY METHODOLOGY

This study was prepared in consultation with the Massachusetts Department of Transportation (MassDOT) and the Town of Wellesley; was performed in accordance with: i) MassDOT's *Transportation Impact Assessment (TIA) Guidelines*; ii) the Traffic Review standards for a Project of Significant Impact (PSI) as defined in Section 5.6 C 3 of the Town of Wellesley Zoning Bylaw; and iii) the standards of the Traffic Engineering and Transportation Planning professions for the preparation of such reports; and was conducted in three distinct stages.

The first stage involved an assessment of existing conditions in the study area and included an inventory of roadway geometrics; pedestrian and bicycle facilities; on-street parking; public transportation services; observations of traffic flow; and collection of pedestrian, bicycle and vehicle counts.

In the second stage of the study, future traffic conditions were projected and analyzed. Specific travel demand forecasts for the Project were assessed along with future traffic demands due to expected traffic growth independent of the Project. A seven-year time horizon was selected for analyses consistent with MassDOT's *Transportation Impact Assessment (TIA) Guidelines*. The traffic analysis conducted in stage two identifies existing or projected future roadway capacity, traffic safety, and site access issues.

The third stage of the study presents and evaluates measures to address traffic and safety issues, if any, identified in stage two of the study.

³*Parking Generation*, 6th Edition; Institute of Transportation Engineers; Washington, D.C.; October 2023. The observed peak parking demand for a multifamily (low-rise) residential building not proximate to rail transit was identified to be 1.27 parking spaces per unit on average with an 85th percentile peak parking demand of 1.59 parking spaces per unit.

EXISTING CONDITIONS

A comprehensive field inventory of existing conditions within the study area was conducted in October and November 2025. The field investigation consisted of an inventory of existing roadway geometrics; pedestrian and bicycle facilities; public transportation services; traffic volumes; and operating characteristics; as well as posted speed limits and land use information within the study area. The study area that was assessed for the Project consisted of Washington Street, and the following specific intersections through which Project-related traffic will travel:

- Washington Street at Grove Street and Central Street
- Washington Street at Church Street
- Washington Street at Weston Road and Denton Road

The following describes the study area roadway and intersections.

ROADWAY

Washington Street (Route 16)

- Two lane urban principal arterial roadway under Town jurisdiction that traverses a general northeast-southwest alignment;
- Provides two 13± foot wide travel lanes separated by a double-yellow centerline with marked on-street parking provided along both sides of the roadway in the vicinity of the Project site;
- A posted speed limit is not provided and, as such, the statutory or “prima facie” speed limit pursuant to M.G.L. c. 90 § 17 is 30 miles per hour (mph);⁴
- Cement concrete sidewalks are provided along both sides of Washington Street in the vicinity of the Project site that include brick accent strips to the north and were observed to be in generally good condition;
- Illumination is provided by way of street lights mounted on ornamental steel poles;

⁴The statutory of “prima facie” speed is defined in M.G.L Chapter 90, Section 17, as the speed which would be deemed reasonable and proper to operate a motor vehicle.

- Land use within the study area consists of the Project site and residential and commercial properties.

INTERSECTIONS

Table 1 and Figure 2 summarize existing lane use, traffic control, and pedestrian and bicycle accommodations at the study area intersections as observed in November 2025.

Table 1
INTERSECTION DESCRIPTION

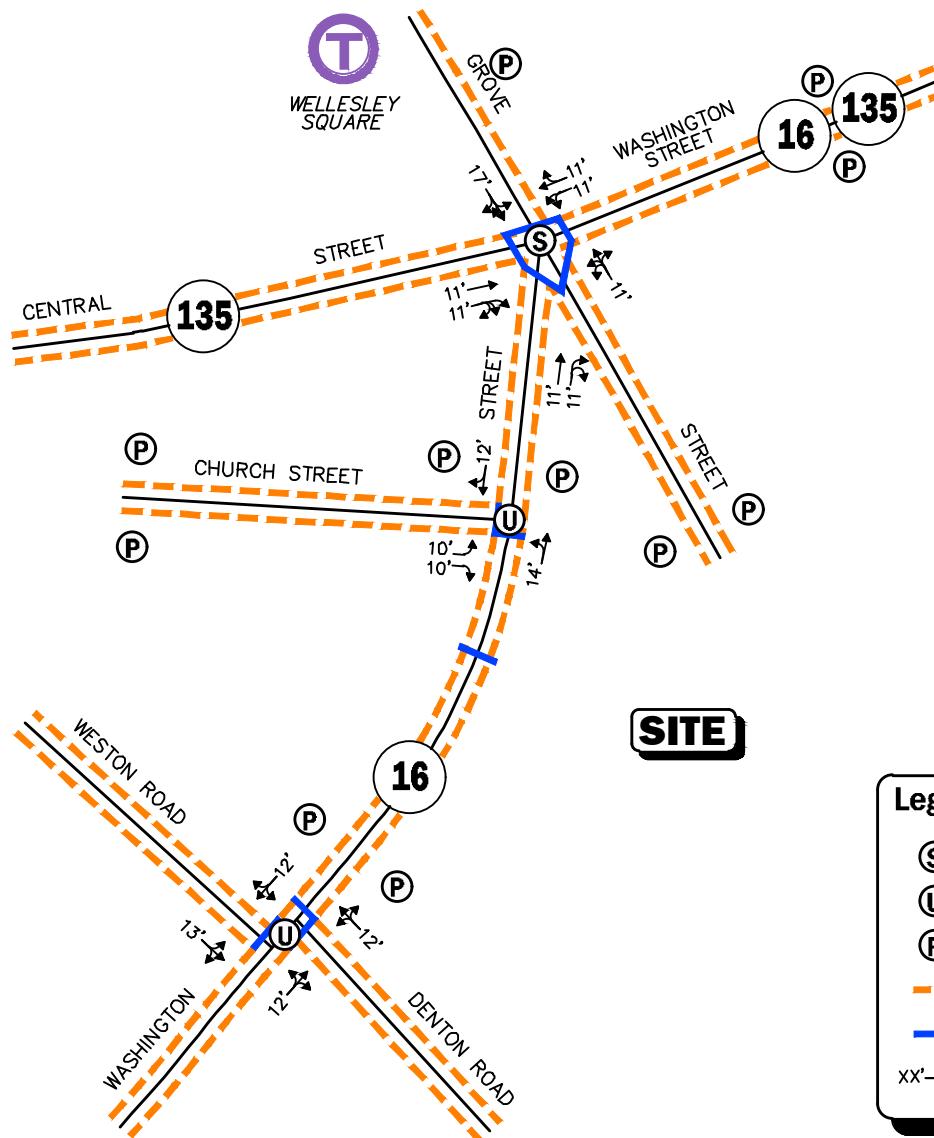
Intersection	Traffic Control Type ^a	No. of Travel Lanes Provided	Shoulder Provided? (Yes/No/Width)	Pedestrian Accommodations? (Yes/No/Description)	Bicycle Accommodations? (Yes/No/Description)
Washington St./Grove St./Central St.	TS	1 left-turn lane and 1 through/right-turn lane on the Washington St. westbound approach; 2 general purpose travel lanes on the Central St. eastbound approach; 2 right-turn lanes on the Washington St. northbound approach; 1 general purpose travel lane on the Grove St. approaches	Yes; 8 feet on Washington St. and Grove St. that serve as parking lanes except where/when prohibited	Yes; sidewalks provided along both sides of the intersecting roadways; crosswalks provided across all legs of the intersection; pedestrian traffic signal equipment and phasing (exclusive) provided	Yes; shared traveled-way ^b
Washington St./Church St.	S	1 general purpose travel lane on Washington St. approaches; separate left and right turn lanes on Church St. approach	Yes; 8 feet on Washington St. and Church St. that serve as parking lanes except where/when prohibited	Yes; sidewalks provided along both sides Washington St. and Church St.; crosswalks provided for crossing Church St. and the Washington St. south leg	Yes; shared traveled-way
Washington St./Weston Rd./Denton Rd.	S	1 general purpose travel lane on all approaches	Yes; 8 feet on Washington St. that serves as a parking lane except where/when prohibited	Yes; sidewalks provided along both sides Washington St. and Denton St. and along the north side of Weston Rd.; crosswalk provided across Weston Rd.	Yes; shared traveled-way

^aTS = traffic signal control; S = stop-sign control.

^bCombined shoulder and travel lane width equal to or exceed 14 feet.

TRAFFIC VOLUMES

In order to determine existing traffic-volume demands and flow patterns within the study area, automatic traffic recorder (ATR) counts, turning movement counts (TMCs) and vehicle classification counts were completed in October 2025. The ATR counts were conducted on Washington Street in the vicinity of the Project site on October 22nd through 23rd, 2025 (Wednesday through Thursday, inclusive) in order to record weekday traffic conditions over an extended period, with weekday morning (7:00 to 9:00 AM) and evening (3:00 to 6:00 PM) peak-period TMCs



Legend:

- Signalized Intersection (S)
- Unsignalized Intersection (U)
- On-Street Parking (P)
- Sidewalk
- Crosswalk
- XX' → Lane Use and Travel Lane Width

Figure 2

Existing Intersection Lane Use, Travel Lane Width, and Pedestrian Facilities

Not To Scale

V Vanasse & Associates inc

performed at the study intersections on Wednesday, October 22, 2025 while public schools and local colleges were in session. These time periods were selected for analysis purposes as they are representative of the peak-traffic-volume hours for both the Project and the adjacent roadway network.

Traffic-Volume Adjustments

In order to evaluate the potential for seasonal fluctuation of traffic volumes within the study area, MassDOT weekday seasonal factors for Urban Group 3 and Groups 4-7 roadways (other principal arterials (Group 3) and minor arterials, major and minor collectors and local roads and streets (Groups 4-7), which include the functional classifications of the study area roadways) were reviewed.⁵ Based on a review of this data, it was determined that traffic volumes for the month of October are between 6.4 and 7.5 percent above average-month conditions. As such, no adjustment was made to the October traffic volumes as they are representative of above average-month conditions.

Based on updated guidance from MassDOT,⁶ adjustments to account for the impact on traffic volumes and trip patterns resulting from the COVID-19 pandemic for traffic counts taken on or after March 1, 2022 are *not recommended* in areas where the adjacent land uses are not predominantly office properties. As the study area roadways and intersections serve a diverse range of land uses (residential and commercial), further adjustment of the traffic-volume data was not required.

The 2025 Existing traffic volumes are summarized in Table 2, with the weekday morning and evening peak-hour traffic volumes graphically depicted on Figure 3. Note that the peak-hour traffic volumes that are presented in Table 2 were obtained from Figure 3.

Table 2
2025 EXISTING TRAFFIC VOLUMES

Location/Peak-Hour	AWT ^a	VPH ^b	K Factor ^c	Directional Distribution ^d
<i>Washington Street, south of #592</i>	9,510	--	--	--
Weekday Morning (7:30 – 8:30 AM)	--	736	7.7	67.5% NB
Weekday Evening (4:15 – 5:15 PM)	--	728	7.7	64.4% SB

^aAverage weekday traffic in vehicles per day.

^bVehicles per hour.

^cPercent of daily traffic occurring during the peak-hour.

^dPercent traveling in peak direction.

NB = northbound; SB = southbound; EB = eastbound; WB = westbound.

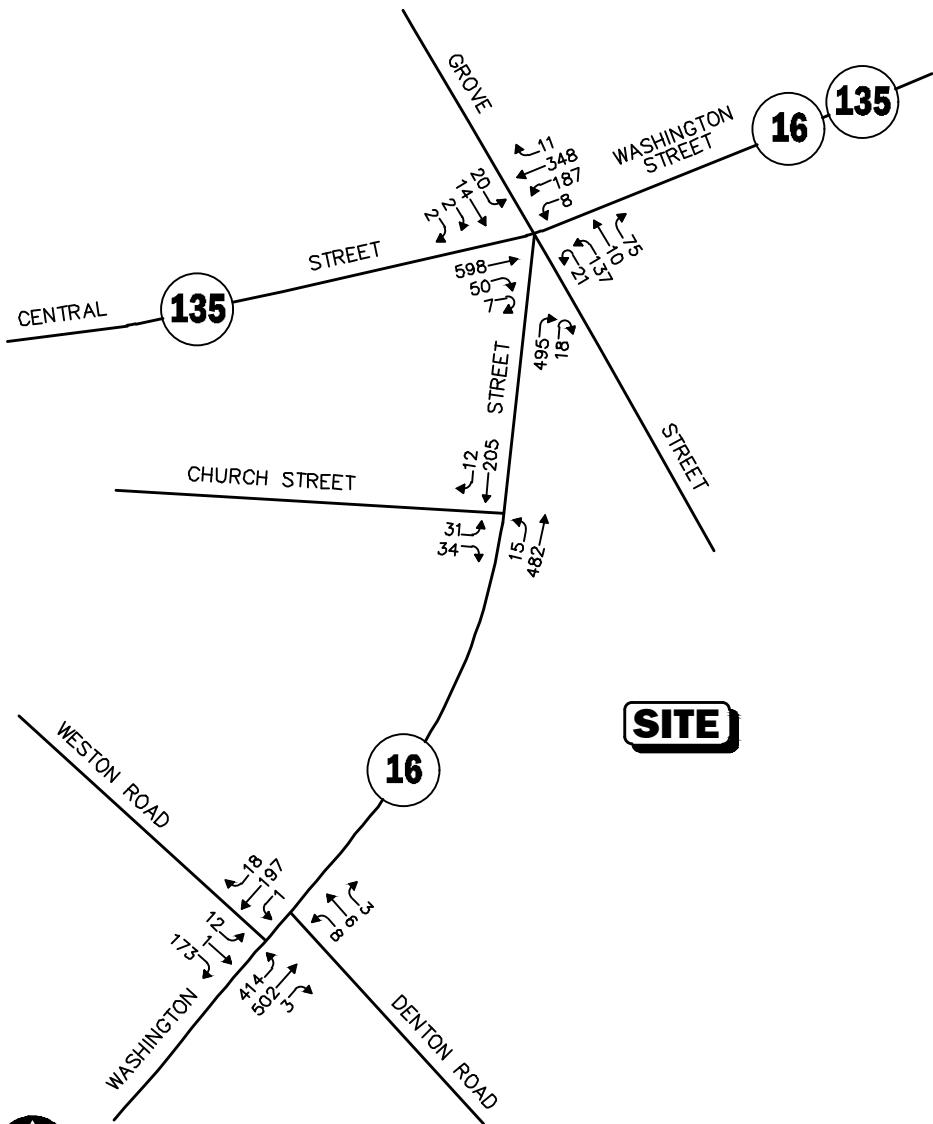
As can be seen in Table 2, Washington Street in the vicinity of the Project site was found to accommodate approximately 9,510 vehicles on an average weekday (two-way, 24-hour volume),

⁵MassDOT Statewide Traffic Data Collection; 2024 Weekday Seasonal Factors, Groups U3 and U4-7.

⁶Traffic and Safety Engineering 25% Design Submission Guidelines; MassDOT; Revised March 31, 2022.

Transportation Impact Assessment - Proposed Multifamily Residential Development - Wellesley, Massachusetts

WEEKDAY MORNING PEAK HOUR (7:30 - 8:30 AM)



Note: Imbalances exist due to numerous curb cuts and side streets that are not shown.

Not To Scale

WEEKDAY EVENING PEAK HOUR (4:15 - 5:15 PM)

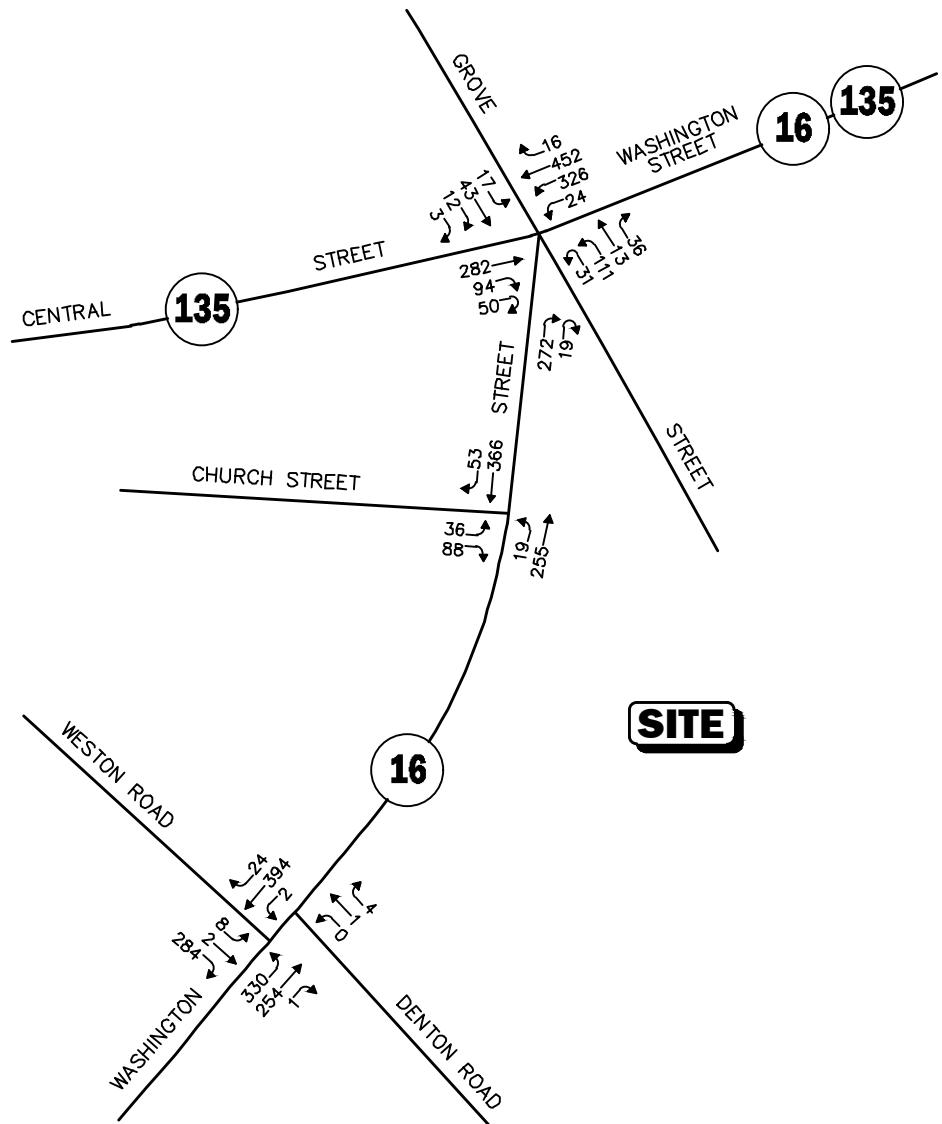


Figure 3

2025 Existing Peak-Hour Traffic Volumes



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with approximately 736 vehicles per hour (vph) during the weekday morning peak-hour and 728 vph during the weekday evening peak-hour.

PEDESTRIAN AND BICYCLE FACILITIES

A comprehensive field inventory of pedestrian and bicycle facilities within the study area was undertaken in November 2025. The field inventory consisted of a review of the location of sidewalks and pedestrian crossing locations along the study roadways and at the study intersections, as well as the location of existing and planned future bicycle facilities.

Pedestrian Facilities

As detailed on Figure 2, sidewalks are generally provided along one or both sides of the study area roadways, with marked crosswalks provided for crossing one or more legs of the study area intersections and the crossings at the Washington Street/Central Street/Grove Street intersection included as a part of the traffic signal system at the intersection (pedestrian pushbuttons, signal indications and phasing are provided for the crossings). A pedestrian actuated Rectangular Rapid Flashing Beacon (RRFB) is provided for crossing the Washington Street south leg of Washington Street/Church Street intersection.

An inventory of sidewalk conditions along Washington Street within 1,000 feet of the Project site indicates that the sidewalks are in generally good condition. Wheelchair ramps are provided for the crossings at the study area intersections; however, many do not include tactile matts as required under the Americans with Disabilities Act (ADA) and several crossings include apex-type ramps that serve more than one crossing, which are also not ADA compliant. The sidewalk along the Project frontage is in good condition and is flush across the driveway.

Bicycle Facilities

Formal bicycle lanes are not provided within the study area; however, the study area roadways generally provides sufficient width to accommodate bicycle travel in a shared traveled-way configuration (i.e., bicyclists and motor vehicles sharing the traveled-way).⁷

PUBLIC TRANSPORTATION

Regularly scheduled public transportation services are not currently provided to the Project site. To the north of the Project site, the Massachusetts Bay Transportation Authority (MBTA) provides Commuter Rail service on the Worcester/Framingham Line between Union Station in Worcester and South Station in Boston, with a stop at Wellesley Square Station, which is an approximate 5-minute walking distance of the Project site. In addition, the MWRSA operates the Catch Connect service within the Town of Wellesley, which is an on-demand, curb-to-curb, microtransit service. The service is booked through the MWRSA CATCH app or by phone. The MBTA also operates The Ride paratransit services for eligible persons who cannot use fixed-route transit all or some of the time due to a physical, cognitive, or mental disability in accordance with ADA requirements.

The public transportation schedules are included in the Appendix.

⁷A minimum combined travel lane and paved shoulder width of 14-feet is required to support bicycle travel in a shared traveled-way condition.

SPOT SPEED MEASUREMENTS

Vehicle travel speed measurements were performed on Washington Street in the vicinity of the Project site in conjunction with the ATR counts. Table 3 summarizes the vehicle travel speed measurements.

Table 3
VEHICLE TRAVEL SPEED MEASUREMENTS

	Washington Street	
	Northbound	Southbound
Mean Travel Speed (mph)	20	23
85 th Percentile Speed (mph)	24	26
Statutory Speed Limit (mph)	30	30

mph = miles per hour.

As can be seen in Table 3, the mean vehicle travel speed along Washington Street in the vicinity of the Project site was found to be 20 mph in the northbound direction and 23 mph southbound. The measured 85th percentile vehicle travel speed, or the speed at which 85 percent of the observed vehicles traveled at or below, was found to be 24 mph in the northbound direction and 26 mph southbound, which is slightly below the statutory speed limit in the vicinity of the Project site (30 mph). The 85th percentile speed is used as the basis of engineering design and in the evaluation of sight distances and is often used in establishing posted speed limits.

MOTOR VEHICLE CRASH DATA

Motor vehicle crash information for the study area intersections was provided by the MassDOT Highway Division Safety Management/Traffic Operations Unit for the most recent five-year period available (2018 through 2022, inclusive) in order to examine motor vehicle crash trends occurring within the study area. The data is summarized by intersection, type, severity, roadway and weather conditions, and day of occurrence, and presented in Table 4.

Table 4
MOTOR VEHICLE CRASH DATA SUMMARY^a

	Washington St./ Central St./ Grove St.	Washington St./ Church St.	Washington St./ 592 Washington St.	Washington St./ Weston Rd./ Denton Rd.
Traffic Control Type: ^b	S	U	U	U
<i>Year:</i>				
2018	7	1	0	1
2019	5	0	0	5
2020	7	2	0	1
2021	5	2	0	1
<u>2022</u>	<u>3</u>	<u>0</u>	<u>0</u>	<u>0</u>
Total	27	5	0	8
Average	5.40	1.00	0.00	1.60
Rate ^c	0.74	0.30	0.00	0.30
MassDOT Crash Rate: ^d	0.78/0.71	0.57/0.52	0.57/0.52	0.57/0.52
Significant? ^e	Yes	No	No	No
<i>Type:</i>				
Angle	6	2	0	2
Rear-End	9	2	0	4
Head-On	1	0	0	0
Sideswipe	8	0	0	0
Fixed Object	2	1	0	2
Pedestrian/Bicycle	1	0	0	0
<u>Unknown/Other</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>
Total	27	5	0	8
<i>Conditions:</i>				
Clear	24	4	0	4
Cloudy	0	0	0	1
Rain	3	1	0	1
<u>Snow/Ice</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>2</u>
Total	27	5	0	8
<i>Lighting:</i>				
Daylight	22	4	0	6
Dawn/Dusk	0	1	0	0
Dark (Road Lit)	5	0	0	2
<u>Dark (Road Unlit)</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>
Total	27	5	0	8
<i>Day of Week:</i>				
Monday through Friday	22	5	0	4
Saturday	2	0	0	1
<u>Sunday</u>	<u>3</u>	<u>0</u>	<u>0</u>	<u>3</u>
Total	27	5	0	8
<i>Severity:</i>				
Property Damage Only	23	5	0	7
Personal Injury	4	0	0	1
Fatality	0	0	0	0
<u>Unknown</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>
Total	27	5	0	8

^aSource: MassDOT Safety Management/Traffic Operations Unit records, 2018 through 2022.

^bTraffic Control Type: S = signalized; U = unsignalized.

^cCrash rate per million vehicles entering the intersection.

^dStatewide/District crash rate.

^eThe intersection crash rate is significant if it is found to exceed the MassDOT crash rate for the MassDOT Highway Division District in which the Project is located (District 6).

As can be seen in Table 4, with the exception of the Washington Street/Grove Street/Central Street intersection, the study area intersections were found to have experienced an average of 1.6 or fewer reported motor vehicle crashes per year over the five-year review period and were found to have motor vehicle crash rates below both the MassDOT statewide and District average crash rates for the MassDOT Highway Division District in which the intersections are located (District 6). The majority of crashes were reported to have occurred on a weekday; during daylight; under clear weather conditions; and involved rear-end-type collisions that resulted in property damage only.

The Washington Street/Grove Street/Central Street intersection was reported to have experienced a total of 27 motor vehicle crashes over the five-year review period and was found to have a motor vehicle crash rate that was *above* the MassDOT District average crash rate for similar intersections but *below* the statewide average crash rate. The majority of crashes occurring at the intersection were reported to have occurred on a weekday; during daylight; under clear weather conditions; and involved rear-end-type or sideswipe collisions that resulted in property damage only. These crashes are most likely attributable to the awkward geometry of the intersection and the associated traffic signal phasing to account for the geometry. Specific recommendations have been provided to enhance safety at the intersection for consideration independent of the Project (see *Recommendations*).

A review of the MassDOT statewide High Crash Location List indicates that there are no locations within the study area that are included on MassDOT's Highway Safety Improvement Program (HSIP) listing as a high crash cluster location. To the northwest of the study area, the Central Street/Weston Road intersection has been defined as a high crash location for the 2019-2021 reporting period and is HSIP eligible. A Road Safety Audit (RSA) has been completed for the intersection that included suggestions to enhance safety at the intersection.⁸

No fatal motor vehicle crashes were reported to have occurred at the study area intersections over the five-year review period.

The detailed MassDOT Crash Rate Worksheets and High Crash Location mapping are provided in the Appendix.

⁸*Road Safety Audit*, Weston Road from Linden Street to Central Street (Route 135) and Central Street (Route 135) from Weston Road to Cross Street, Town of Wellesley; Toole Design; August 8, 2023

FUTURE CONDITIONS

Traffic volumes in the study area were projected to the year 2032, which reflects a seven-year planning horizon consistent with MassDOT's *Transportation Impact Assessment (TIA) Guidelines*. Independent of the Project, traffic volumes on the roadway network in the year 2032 under No-Build conditions include all existing traffic and new traffic resulting from background traffic growth. Anticipated Project-generated traffic volumes superimposed upon the 2032 No-Build traffic volumes reflect 2032 Build traffic-volume conditions with the Project.

FUTURE TRAFFIC GROWTH

Future traffic growth is a function of the expected land development in the immediate area and the surrounding region. Several methods can be used to estimate this growth. A procedure frequently employed estimates an annual percentage increase in traffic growth and applies that percentage to all traffic volumes under study. The drawback to such a procedure is that some turning volumes may actually grow at either a higher or a lower rate at particular intersections.

An alternative procedure identifies the location and type of planned development, estimates the traffic to be generated, and assigns it to the area roadway network. This procedure produces a more realistic estimate of growth for local traffic; however, potential population growth and development external to the study area would not be accounted for in the resulting traffic projections.

To provide a conservative analysis framework, both procedures were used, the salient components of which are described below.

Specific Development by Others

The Town of Wellesley Planning Department was contacted in order to determine if there were any projects planned within the study area that would have an impact on future traffic volumes at the study intersections. Based on these discussions, the following projects were identified for review in conjunction with the development of the future traffic volume projections:

- ***Multifamily Affordable Units, 140 Weston Road, Wellesley, Massachusetts.*** This development entails the construction of two (2) affordable units at 140 Weston Road to the northwest of the Project site. Traffic volumes associated with this development within the

study area of this assessment are expected to be relatively minor and would be accounted for in the general background traffic growth rate.

In addition to the above mentioned development, the Planning Department indicated that potential developments could occur at the following parcels proximate to the Project site: 540-568 Washington Street; 570-574 Washington Street; and 40 Grove Street; however, there are no proposals before the Town at the time of the preparation of this assessment. No other specific development projects by others were identified at this time that are expected to result in an increase in traffic volumes that would exceed the general background traffic growth rate (discussion follows).

General Background Traffic Growth

Traffic-volume data compiled by MassDOT from permanent count stations located in Wellesley and Newton were reviewed in order to determine general traffic growth trends in the area. This data indicates that annual traffic volumes have fluctuated over the past several years, with the average growth rate found to be approximately 0.87 percent per year. As such, a slightly higher 1.0 percent per year compounded annual background traffic growth rate was used in order to account for future traffic growth and presently unforeseen development within the study area.

Roadway Improvement Projects

The Town of Wellesley and MassDOT were contacted in order to determine if there were any planned future roadway improvement projects expected to be completed by 2032 within the study area. Based on these discussions, no roadway improvement projects aside from routine maintenance activities were identified to be planned within the study area at this time.

No-Build Traffic Volumes

The 2032 No-Build condition peak-hour traffic-volumes were developed by applying the 1.0 percent per year compounded annual background traffic growth rate to the 2025 Existing peak-hour traffic volumes. The resulting 2031 No-Build weekday morning and evening peak-hour traffic volumes are shown on Figure 4.

PROJECT-GENERATED TRAFFIC

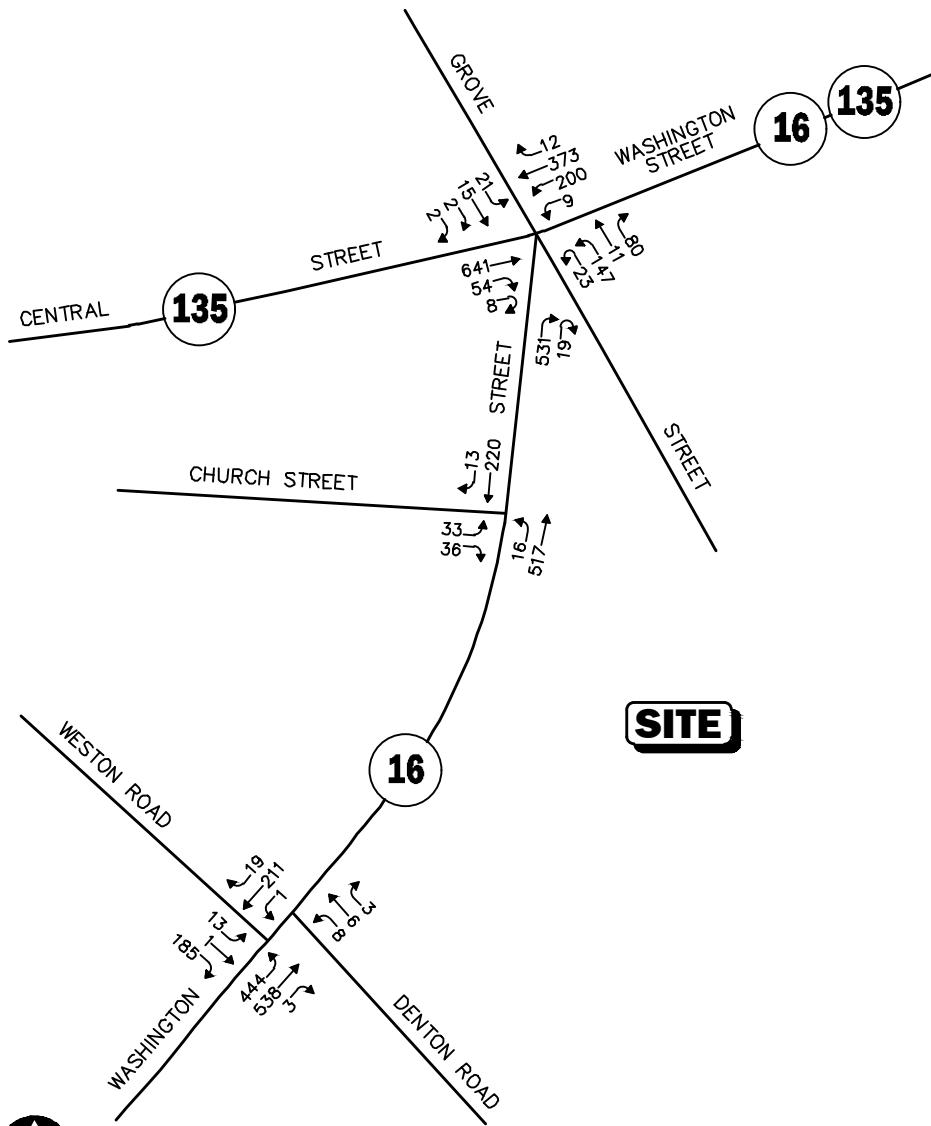
Design year (2032 Build) traffic volumes for the study area roadways were determined by estimating Project-generated traffic volumes and assigning those volumes on the study roadways. The following sections describe the methodology used to develop the anticipated traffic characteristics of the Project.

As proposed, the Project will entail the construction of a three-story, 19-unit, multifamily residential building. In order to develop the traffic characteristics of the Project, trip generation statistics published by the Institute of Transportation Engineers (ITE)⁹ for a similar land use as that proposed was used. ITE Land Use Code 220, *Multifamily Housing (Low-Rise)*, was used to establish the base trip-generation calculations for the Project.

⁹Institute of Transportation Engineer, op. cit. 1.

Transportation Impact Assessment - Proposed Multifamily Residential Development - Wellesley, Massachusetts

WEEKDAY MORNING PEAK HOUR (7:30 - 8:30 AM)



Note: Imbalances exist due to numerous curb cuts and side streets that are not shown.

Not To Scale



WEEKDAY EVENING PEAK HOUR (4:15 - 5:15 PM)

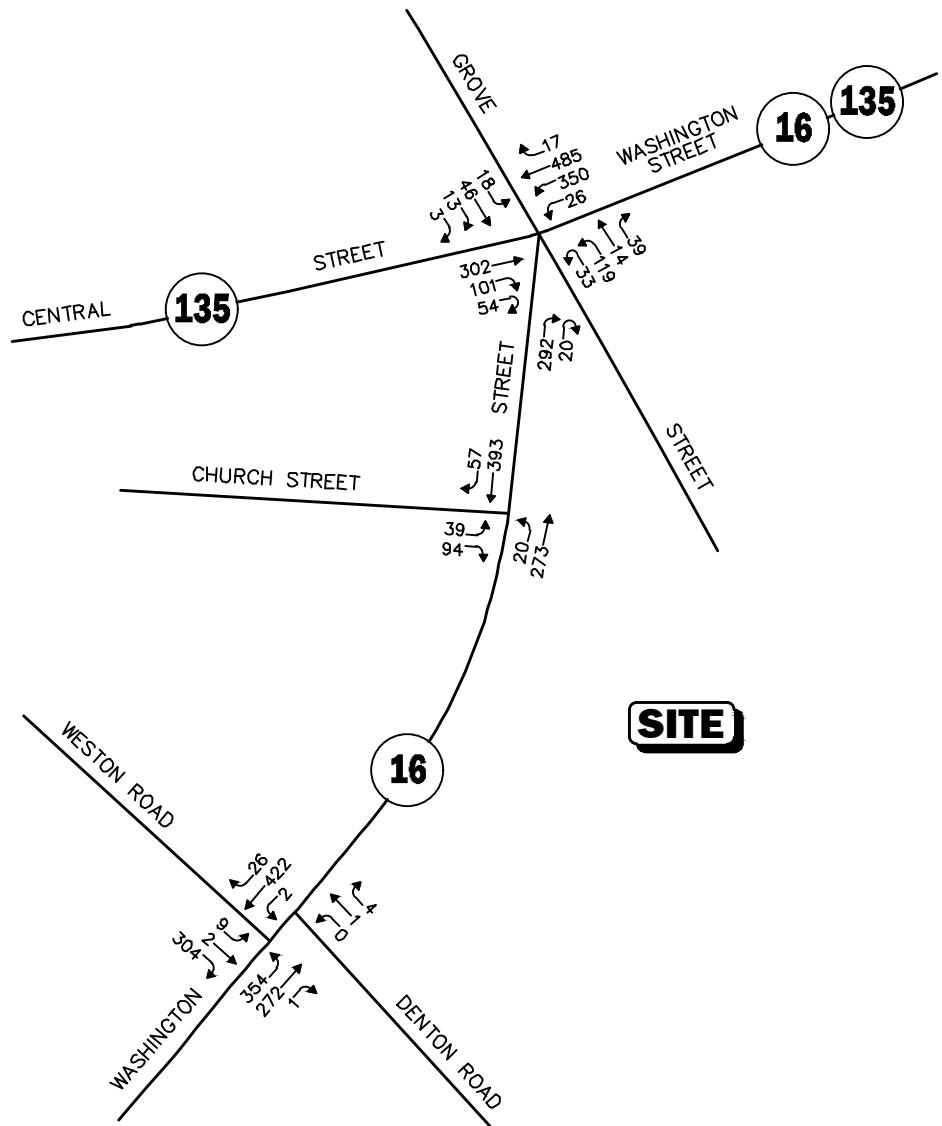


Figure 4

2032 No-Build
Peak-Hour Traffic Volumes

Table 5 summarizes the anticipated traffic characteristics of the Project using the above methodology without consideration of residents that may walk or bike to the Wellesley Square Commuter Rail Station or to a use within Wellesley Square. Consideration of such trips would reduce the overall volume of traffic produced by the Project from those shown in Table 5.

Table 5
TRIP-GENERATION SUMMARY^a

Time Period	Vehicle Trips ^a		
	Entering	Exiting	Total
<i>Average Weekday:</i>	114	114	228
<i>Weekday Morning Peak-Hour:</i>	5	15	20
<i>Weekday Evening Peak-Hour:</i>	10	6	16

^aBased on ITE LUC 220, *Multifamily Housing (Low-Rise)* (19 dwelling units).

Project-Generated Traffic-Volume Summary

As can be seen in Table 5, without adjustment to account for the use of alternative modes of transportation to Single-Occupant Vehicles (SOVs), the Project is expected to generate approximately 228 vehicle trips on an average weekday (two-way, 24-hour volume, or 114 vehicles entering and 114 exiting), with 20 vehicle trips expected during the weekday morning peak-hour (5 vehicles entering and 15 exiting) and 16 vehicle trips expected during the weekday evening peak-hour (10 vehicles entering and 6 exiting).

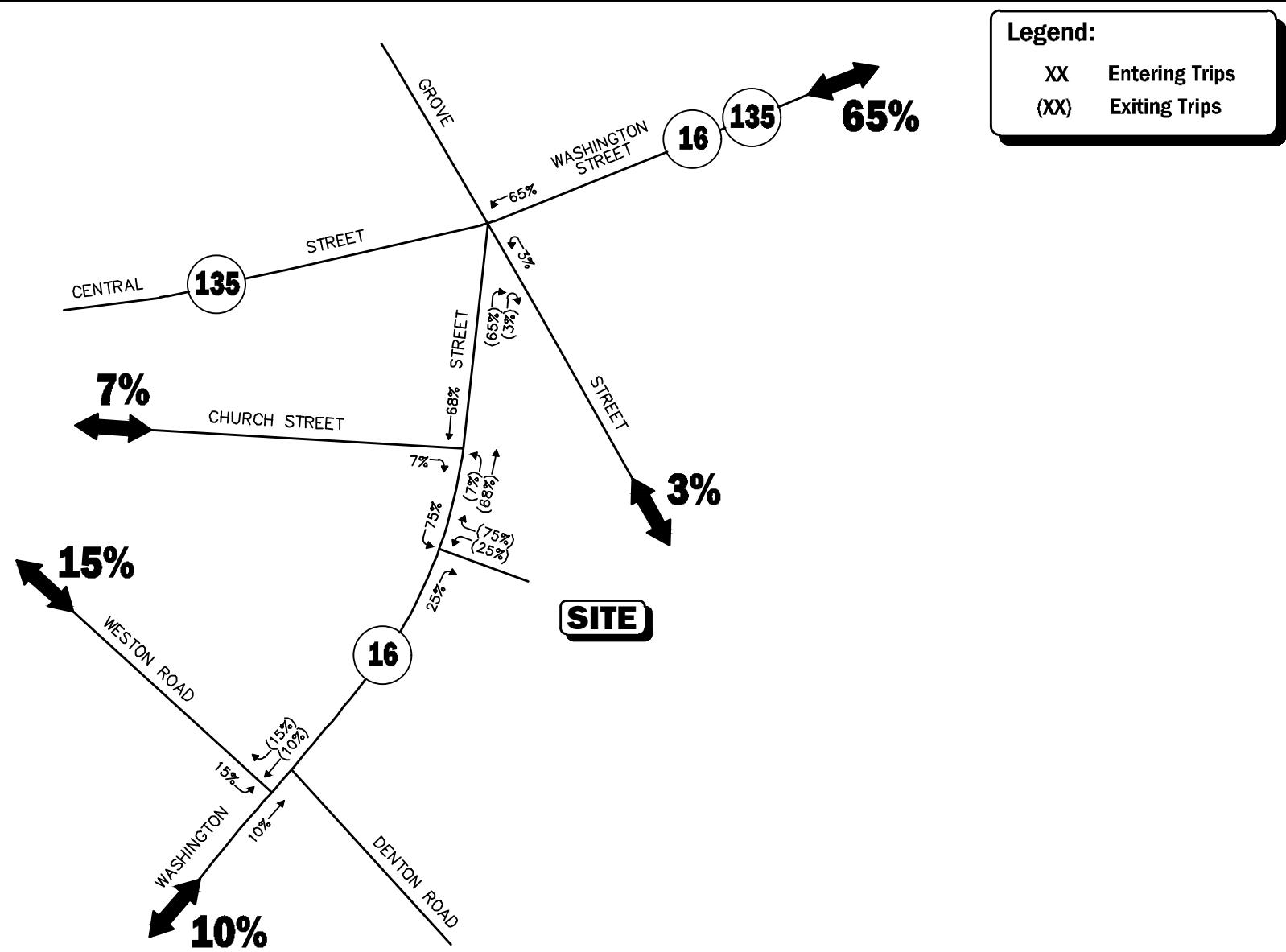
TRIP DISTRIBUTION AND ASSIGNMENT

The directional distribution of generated trips to and from the Project site was determined based on a review of Journey-to-Work data obtained from the U.S. Census for the Town of Wellesley and refined using existing traffic patterns within the study area. The general trip distribution for the Project is graphically depicted on Figure 5. The additional traffic expected to be generated by the Project was assigned on the study area roadway network as shown on Figure 6 for the weekday morning and evening peak hours.

FUTURE TRAFFIC VOLUMES – BUILD CONDITION

The 2032 Build condition traffic volumes consist of the 2032 No-Build traffic volumes with the additional traffic expected to be generated by the Project added to them. The 2032 Build weekday morning and evening peak-hour traffic volumes are graphically depicted on Figure 7.

A summary of peak-hour projected traffic-volume changes outside of the study area that is the subject of this assessment is shown in Table 6. These changes are a result of the construction of the Project.



Not To Scale

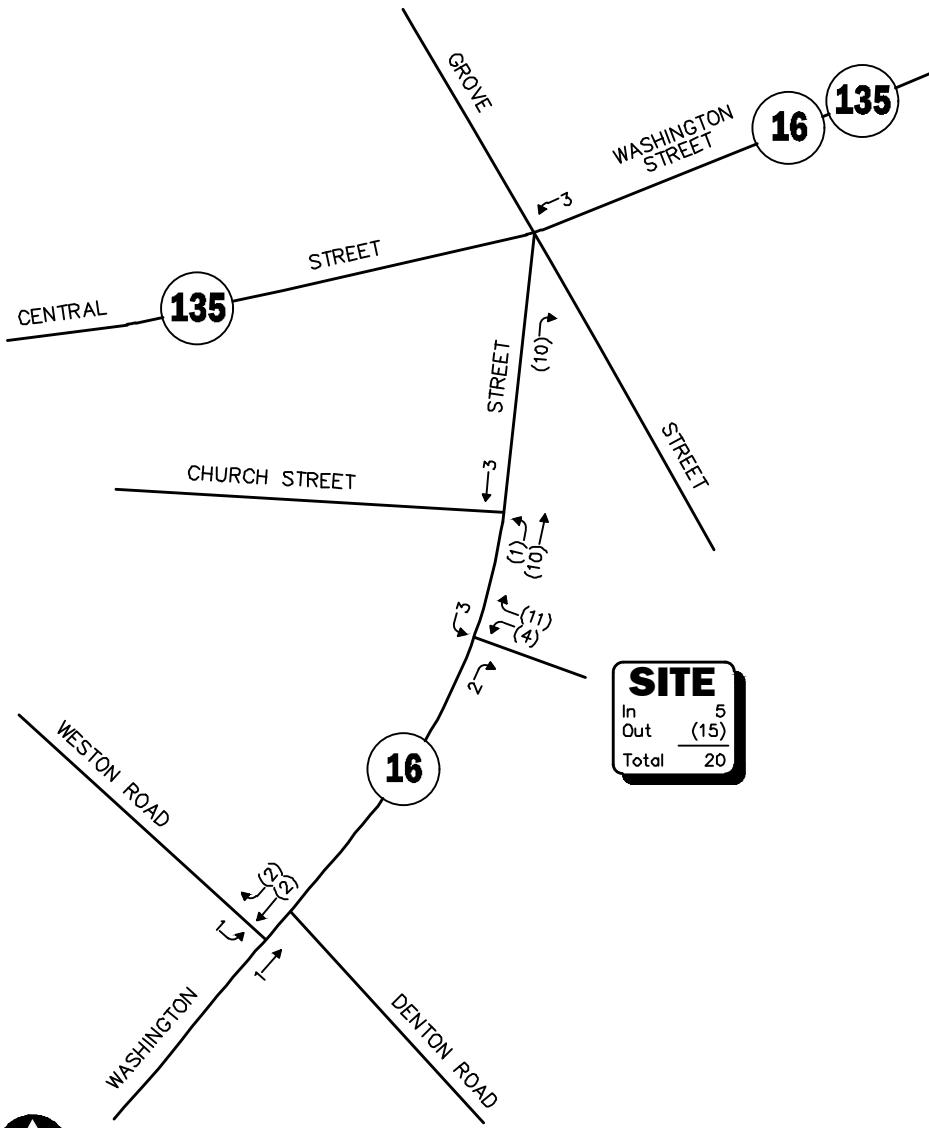
V Vanasse & Associates inc

Figure 5

Trip Distribution Map

Transportation Impact Assessment - Proposed Multifamily Residential Development - Wellesley, Massachusetts

WEEKDAY MORNING PEAK HOUR (7:30 - 8:30 AM)



Not To Scale

WEEKDAY EVENING PEAK HOUR (4:15 - 5:15 PM)

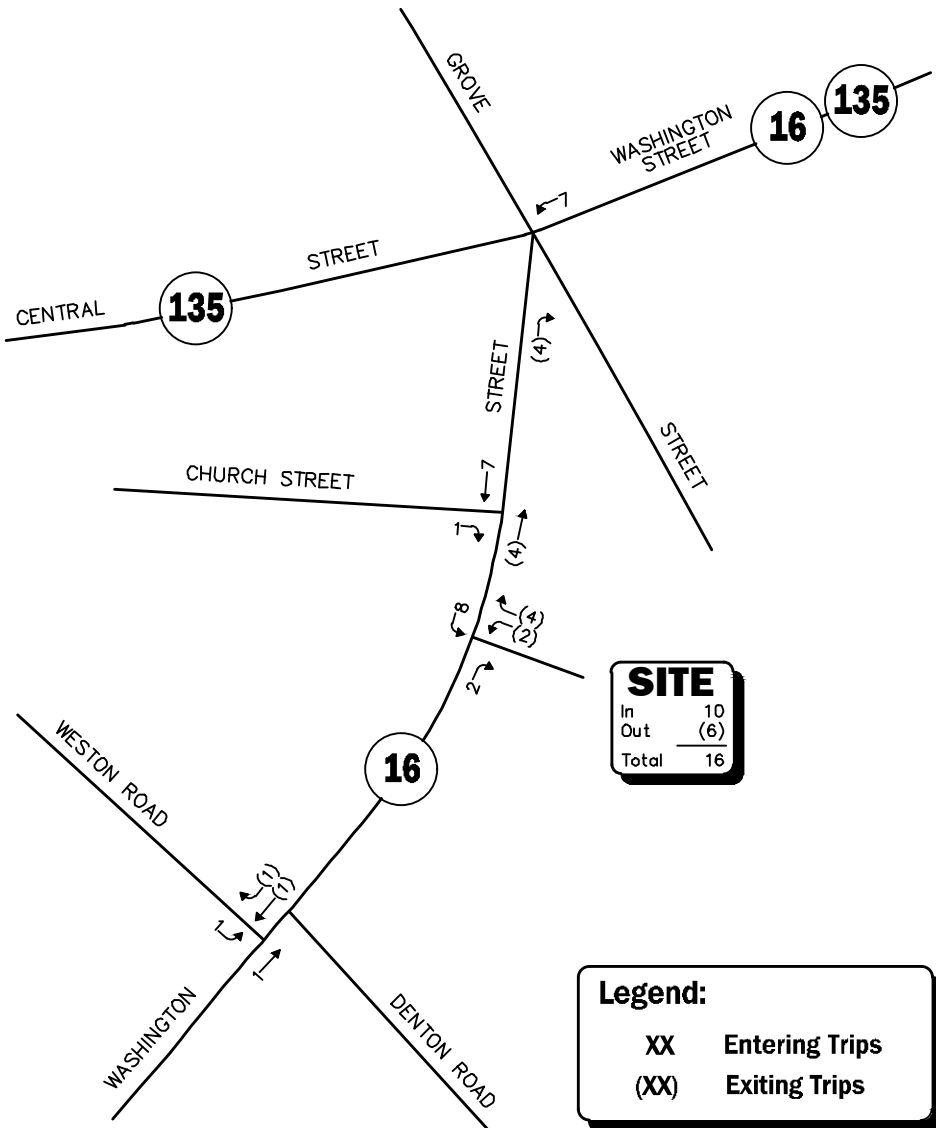
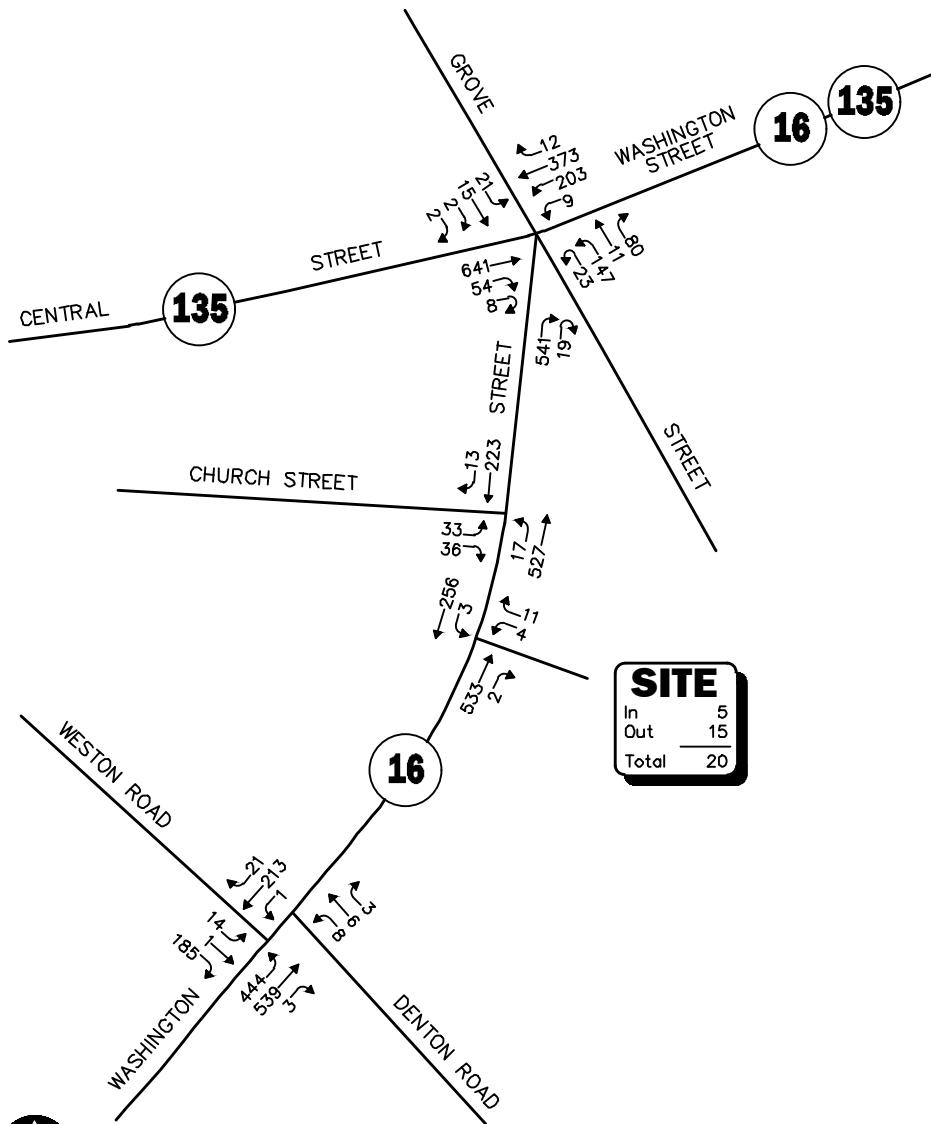


Figure 6

Project-Generated
Peak-Hour Traffic Volumes

Transportation Impact Assessment - Proposed Multifamily Residential Development - Wellesley, Massachusetts

WEEKDAY MORNING PEAK HOUR (7:30 - 8:30 AM)



Note: Imbalances exist due to numerous curb cuts and side streets that are not shown.

Not To Scale

WEEKDAY EVENING PEAK HOUR (4:15 - 5:15 PM)

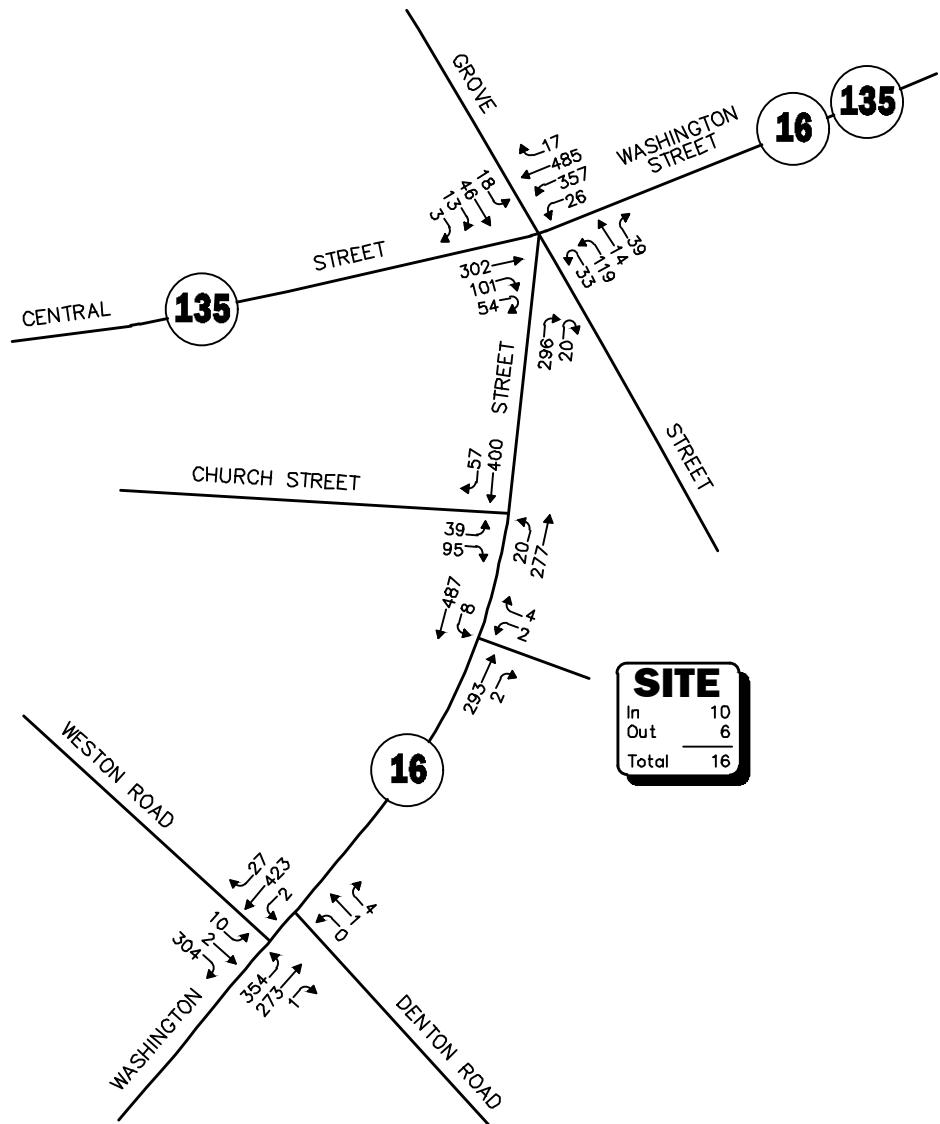


Figure 7

2032 Build
Peak-Hour Traffic Volumes

Table 6
PEAK-HOUR TRAFFIC-VOLUME INCREASES

Location/Peak-Hour	2025 Existing	2032 No-Build	2032 Build	Traffic-Volume Increase Over No-Build	Percent Increase Over No-Build
<i>Washington Street, east of Grove St.:</i>					
Weekday Morning	1,742	1,846	1,859	13	0.7
Weekday Evening	1,425	1,511	1,522	11	0.7
<i>Washington Street, south of Weston Road:</i>					
Weekday Morning	1,297	1,389	1,392	3	0.2
Weekday Evening	1,263	1,353	1,355	2	0.1
<i>Church Street, west of Washington St.:</i>					
Weekday Morning	92	98	99	1	0.1
Weekday Evening	196	210	211	1	0.1
<i>Weston Road, west of Washington Street:</i>					
Weekday Morning	624	668	671	3	0.4
Weekday Evening	649	696	698	2	0.3

As shown in Table 6, Project-related traffic-volume changes outside of the study area relative to 2032 No-Build conditions are anticipated to range from increases of 0.1 to 0.7 percent during the peak periods, with vehicle increases shown to range from 1 to 13 vehicles. ***When distributed over the peak-hour, the predicted traffic-volume increases would not result in a material impact (increase) on motorist delays or vehicle queuing outside of the immediate study area that is the subject of this assessment.***

TRAFFIC OPERATIONS ANALYSIS

Measuring existing and future traffic volumes quantifies traffic flow within the study area. To assess quality of flow, roadway capacity and vehicle queue analyses were conducted under Existing, No-Build, and Build traffic-volume conditions. Capacity analyses provide an indication of how well the roadway facilities serve the traffic demands placed upon them, with vehicle queue analyses providing a secondary measure of the operational characteristics of an intersection or section of roadway under study.

METHODOLOGY

Levels of Service

A primary result of capacity analyses is the assignment of level of service to traffic facilities under various traffic-flow conditions.¹⁰ The concept of level of service is defined as a qualitative measure describing operational conditions within a traffic stream and their perception by motorists and/or passengers. A level-of-service definition provides an index to quality of traffic flow in terms of such factors as speed, travel time, freedom to maneuver, traffic interruptions, comfort, convenience, and safety.

Six levels of service are defined for each type of facility. They are given letter designations from A to F, with level-of-service (LOS) A representing the best operating conditions and LOS F representing congested or constrained operating conditions.

Since the level of service of a traffic facility is a function of the traffic flows placed upon it, such a facility may operate at a wide range of levels of service, depending on the time of day, day of week, or period of year.

¹⁰The capacity analysis methodology is based on the concepts and procedures presented in the *Highway Capacity Manual, 6th Edition*; Transportation Research Board; Washington, DC; 2016.

Signalized Intersections

The six levels of service for signalized intersections may be described as follows:

- *LOS A* describes operations with very low control delay; most vehicles do not stop at all.
- *LOS B* describes operations with relatively low control delay. However, more vehicles stop than LOS A.
- *LOS C* describes operations with higher control delays. Individual cycle failures may begin to appear. The number of vehicles stopping is significant at this level, although many still pass through the intersection without stopping.
- *LOS D* describes operations with control delay in the range where the influence of congestion becomes more noticeable. Many vehicles stop and individual cycle failures are noticeable.
- *LOS E* describes operations with high control delay values. Individual cycle failures are frequent occurrences.
- *LOS F* describes operations with high control delay values that often occur with over-saturation. Poor progression and long cycle lengths may also be major contributing causes to such delay levels.

Levels of service for signalized intersections are calculated using the operational analysis methodology of the 2000 *Highway Capacity Manual*¹¹ and implemented as a part of the Synchro® 12 software. This method assesses the effects of signal type, timing, phasing, and progression; vehicle mix; and geometrics on delay. Level-of-service designations are based on the criterion of control or signal delay per vehicle. Control or signal delay is a measure of driver discomfort, frustration, and fuel consumption, and includes initial deceleration delay approaching the traffic signal, queue move-up time, stopped delay and final acceleration delay. Table 7 summarizes the relationship between level of service and control delay. The tabulated control delay criterion may be applied in assigning level-of-service designations to individual lane groups, to individual intersection approaches, or to entire intersections.

Table 7
LEVEL-OF-SERVICE CRITERIA
FOR SIGNALIZED INTERSECTIONS^a

Level of Service	Control (Signal) Delay Per Vehicle (Seconds)
A	≤ 10.0
B	10.1 to 20.0
C	20.1 to 35.0
D	35.1 to 55.0
E	55.1 to 80.0
F	>80.0

^aSource: *Highway Capacity Manual*, Transportation Research Board; Washington, DC; 2000; page 16-2.

¹¹*Highway Capacity Manual*; Transportation Research Board; Washington, DC; 2000.

Unsignalized Intersections

The six levels of service for unsignalized intersections may be described as follows:

- *LOS A* represents a condition with little or no control delay to minor street traffic.
- *LOS B* represents a condition with short control delays to minor street traffic.
- *LOS C* represents a condition with average control delays to minor street traffic.
- *LOS D* represents a condition with long control delays to minor street traffic.
- *LOS E* represents operating conditions at or near capacity level, with very long control delays to minor street traffic.
- *LOS F* represents a condition where minor street demand volume exceeds the capacity of an approach lane, with extreme control delays resulting.

The levels of service of unsignalized intersections are determined by application of a procedure described in the *Highway Capacity Manual 7th Edition*.¹² Level of service is measured in terms of average control delay. Mathematically, control delay is a function of the capacity and degree of saturation of the lane group and/or approach under study and is a quantification of motorist delay associated with traffic control devices such as traffic signals and STOP signs. Control delay includes the effects of initial deceleration delay approaching a STOP sign, stopped delay, queue move-up time, and final acceleration delay from a stopped condition. Definitions for level of service at unsignalized intersections are also given in the *Highway Capacity Manual 7th Edition*. Table 8 summarizes the relationship between level of service and average control delay for two-way stop controlled and all-way stop controlled intersections.

Table 8
LEVEL-OF-SERVICE CRITERIA FOR
UNSIGNALED INTERSECTIONS^a

Level-Of-Service by Volume-to-Capacity Ratio		Average Control Delay (Seconds Per Vehicle)
v/c ≤ 1.0	v/c > 1.0	
A	F	≥10.0
B	F	10.1 to 15.0
C	F	15.1 to 25.0
D	F	25.1 to 35.0
E	F	35.1 to 50.0
F	F	>50.0

^aSource: *Highway Capacity Manual*; Transportation Research Board; Washington, DC; 2023.

¹²*Highway Capacity Manual*; Transportation Research Board; Washington, DC; 2023.

Vehicle Queue Analysis

Vehicle queue analyses are a direct measurement of an intersection's ability to process vehicles under various traffic control and volume scenarios and lane use arrangements. The vehicle queue analysis was performed using the Synchro® intersection capacity analysis software. The Synchro® vehicle queue analysis methodology is a simulation based model which reports the number of vehicles that experience a delay of six seconds or more at an intersection. For signalized intersections, Synchro® reports both the average (50th percentile) and the 95th percentile vehicle queue. For unsignalized intersections, Synchro® reports the 95th percentile vehicle queue. Vehicle queue lengths are a function of the capacity of the movement under study and the volume of traffic being processed by the intersection during the analysis period. The 95th percentile vehicle queue is the vehicle queue length that will be exceeded only 5 percent of the time, or approximately three minutes out of sixty minutes during the peak one hour of the day (during the remaining fifty-seven minutes, the vehicle queue length will be less than the 95th percentile queue length).

ANALYSIS RESULTS

Level-of-service and vehicle queue analyses were conducted for 2025 Existing, 2032 No-Build, and 2032 Build conditions for the intersections within the study area. The results of the intersection capacity and vehicle queue analyses are summarized in Tables 9 and 10, with the detailed analysis results presented in the Appendix.

The following is a summary of the level-of-service and vehicle queue analyses for the intersections within the study area. For context, we note that an LOS of "D" or better is generally defined as "acceptable" operating conditions. Project-related impacts at the study area intersections were identified as follows:

Signalized Intersection (Table 9)

Washington Street at Central Street and Grove Street

No change in level-of-service is predicted to occur for any movement over No-Build conditions, with Project-related impacts generally defined as an increase in overall average motorist delay of up to 3.7 seconds that resulted in a corresponding increase in vehicle queuing of up to one (1) vehicle. Independent of the Project, one or more movements at the intersection are currently operating at or over capacity (i.e., LOS "E" or "F", respectively).

Unsignalized Intersections (Table 10)

Washington Street at Church Street

No change in level-of-service or vehicle queuing is predicted to occur for any movement over No-Build conditions, with Project-related impacts generally defined as an increase in average motorist delay of less than 1.0 seconds. Actual operating conditions at this intersection are directly related to vehicle queuing along the Washington Street northbound approach to the Washington Street/Central Street/Grove Street intersection during the peak periods.

Washington Street at Weston Road and Denton Road

No change in level-of-service for any movement is predicted to occur over No-Build conditions, with Project-related impacts generally defined as a predicted increase in average motorist delay that resulted in a corresponding increase in vehicle queuing of up to one (1) vehicle. Independent of the Project, all movements from Weston Road and Denton Road are currently or are predicted to operate over capacity during the peak periods.

Washington Street at the Project Site Driveway

All movements exiting the Project site driveway to Washington Street are predicted to operate at LOS B during both peak hours with negligible vehicle queuing predicted. All movements along Washington Street approaching the driveway are predicted to operate at LOS A, also with negligible vehicle queuing. Actual operating conditions at this intersection are directly related to vehicle queuing along the Washington Street northbound approach to the Washington Street/Central Street/Grove Street intersection during the peak periods.

Table 9
SIGNALIZED INTERSECTION LEVEL-OF-SERVICE AND VEHICLE QUEUE SUMMARY

Signalized Intersection/ Peak-Hour/Movement	2025 Existing				2032 No-Build				2032 Build			
	V/C ^a	Delay ^b	LOS ^c	Queue ^d 50 th /95 th	V/C	Delay	LOS	Queue ^d 50 th /95 th	V/C	Delay	LOS	Queue ^d 50 th /95 th
Washington St. at Central St. and Grove St.												
<i>Weekday Morning:</i>												
Central St. EB TH/RT	0.74	29.4	D	6/12	0.75	29.4	C	6/14	0.75	29.4	C	6/14
Washington St. WB LT	1.21	>80.0	F	5/15	1.38	>80.0	F	6/16	1.40	>80.0	F	6/16
Washington St. WB TH/RT	0.39	9.2	A	3/11	0.41	9.2	A	4/12	0.41	9.2	A	4/12
Washington St. NB RT	0.82	35.1	D	6/15	0.89	43.5	D	6/17	0.91	45.9	D	7/17
Grove St. SEB LT/TH/RT	0.13	24.6	C	1/2	0.15	25.6	C	1/2	0.15	25.6	C	1/2
Grove St. NWB LT/TH/RT	0.94	66.8	E	5/15	1.03	>80.0	F	6/16	1.03	>80.0	F	6/16
Overall	--	45.0	D	--	--	57.4	E	--	--	59.2	E	--
<i>Weekday Evening:</i>												
Central St. EB TH/RT	0.65	30.9	C	4/8	0.68	31.2	C	4/9	0.68	31.2	C	4/9
Washington St. WB LT	1.70	>80.0	F	9/25	1.93	>80.0	F	11/27	1.96	>80.0	F	11/28
Washington St. WB TH/RT	0.53	12.9	B	4/14	0.56	13.3	B	5/16	0.56	13.3	B	5/16
Washington St. NB RT	0.45	25.8	C	3/7	0.49	26.6	C	3/7	0.50	26.7	C	3/7
Grove St. SEB LT/TH/RT	0.19	25.5	C	1/4	0.20	26.0	C	1/4	0.20	26.0	C	1/4
Grove St. NWB LT/TH/RT	0.76	40.1	D	4/11	0.84	49.0	D	4/13	0.84	49.0	D	4/13
Overall	--	88.9	F	--	--	109.7	F	--	--	113.4	F	--

^aVolume-to-capacity ratio.

^bControl (signal) delay per vehicle in seconds.

^cLevel-of-Service.

^dQueue length in vehicles based on 25-feet per vehicle.

NB = northbound; SB = southbound; WB = westbound; EB = eastbound; NWB = northwestbound; SEB = southeastbound.

LT = left-turning movements; TH = through movements; RT = right-turning movements.

Table 10
UNSIGNALIZED INTERSECTION LEVEL-OF-SERVICE AND VEHICLE QUEUE SUMMARY

Unsignalized Intersection/ Peak-Hour/Movement	2025 Existing				2032 No-Build				2032Build			
	Demand ^a	Delay ^b	LOS ^c	Queue ^d 95 th	Demand	Delay	LOS	Queue ^d 95 th	Demand	Delay	LOS	Queue ^d 95 th
Washington St. at Church St.												
<i>Weekday Morning:</i>												
Church St. EB LT	31	17.4	C	1	33	18.8	C	1	33	19.2	C	1
Church St. EB RT	34	10.2	B	0	36	10.4	B	0	36	10.4	B	0
Washington St. NB LT/TH	497	0.2	A	0	533	0.2	A	0	544	0.3	A	0
Washington St. SB TH/RT	217	0.0	A	0	233	0.0	A	0	236	0.0	A	0
<i>Weekday Evening:</i>												
Church St. EB LT	36	17.7	C	1	39	19.0	C	1	39	19.3	C	1
Church St. EB RT	88	13.4	B	1	94	13.9	B	1	95	14.0	B	1
Washington St. NB LT/TH	274	0.6	A	0	293	0.6	A	0	297	0.6	A	0
Washington St. SB TH/RT	419	0.0	A	0	450	0.0	A	0	457	0.0	A	0
Washington St. at Weston Rd. and Denton Rd.												
<i>Weekday Morning:</i>												
Weston Rd. EB LT/TH/RT	186	>50.0	F	9	199	>50.0	F	15	200	>50.0	F	16
Denton Rd. WB LT/TH/RT	17	>50.0	F	3	17	>50.0	F	3	17	>50.0	F	3
Washington St. NB LT/TH/RT	919	4.3	A	2	985	4.4	A	2	986	4.5	A	2
Washington St. SB LT/TH/RT	216	0.0	A	0	231	0.0	A	0	235	0.0	A	0
<i>Weekday Evening:</i>												
Weston Rd. EB LT/TH/RT	294	35.0	D	6	315	>50.0	F	9	316	>50.0	F	10
Denton Rd. WB LT/TH/RT	5	22.2	C	0	5	25.9	D	1	5	26.0	D	1
Washington St. NB LT/TH/RT	585	5.7	A	2	627	6.0	A	2	628	6.0	A	2
Washington St. SB LT/TH/RT	420	0.0	A	0	450	0.0	A	0	452	0.0	A	0
Washington St. at the Project Site Driveway												
<i>Weekday Morning:</i>												
Project Site Driveway WB LT/RT	--	--	--	--	--	--	--	--	15	13.6	B	0
Washington St. NB TH/RT	--	--	--	--	--	--	--	--	535	0.0	A	0
Washington St. SB LT/TH	--	--	--	--	--	--	--	--	259	0.1	A	0
<i>Weekday Evening:</i>												
Worcester Street EB RT	--	--	--	--	--	--	--	--	6	12.3	B	0
Project Site Driveway NB LT/RT	--	--	--	--	--	--	--	--	295	0.0	A	0
Worcester Street WB LT/TH	--	--	--	--	--	--	--	--	495	0.1	A	0

^aDemand in vehicles per hour.

^bAverage control delay per vehicle (in seconds).

^cLevel-of-Service.

^dQueue length in vehicles.

NB = northbound; SB = southbound; EB = eastbound; WB = westbound.

LT = left-turning movements; TH = through movements; RT = right-turning movements.

SIGHT DISTANCE EVALUATION

Sight distance measurements were performed at the Project site driveway intersection with Washington Street in accordance with MassDOT and American Association of State Highway and Transportation Officials (AASHTO)¹³ requirements. Both stopping sight distance (SSD) and intersection sight distance (ISD) measurements were performed. In brief, SSD is the distance required by a vehicle traveling at the design speed of a roadway, on wet pavement, to stop prior to striking an object in its travel path. ISD or corner sight distance (CSD) is the sight distance required by a driver entering or crossing an intersecting roadway to perceive an on-coming vehicle and safely complete a turning or crossing maneuver with on-coming traffic. In accordance with AASHTO standards, if the measured ISD is at least equal to the required SSD value for the appropriate design speed, the intersection can operate in a safe manner. Table 11 presents the measured SSD and ISD at the subject intersection.

¹³ *A Policy on Geometric Design of Highway and Streets*, 7th Edition; American Association of State Highway and Transportation Officials (AASHTO); Washington D.C.; 2018.

Table 11
SIGHT DISTANCE MEASUREMENTS^a

Intersection/Sight Distance Measurement	Feet		
	Required Minimum (SSD)	Desirable (ISD) ^b	Measured
<i>Washington Street at the Project Site Driveway</i>			
<i>Stopping Sight Distance:</i>			
Washington Street approaching from the north	200	--	335
Washington Street approaching from the south	200	--	386
<i>Intersection Sight Distance:</i>			
Looking to the north from the Project Site Driveway	200	335	300+
Looking to the south from the Project Site Driveway	200	290	400+

^aRecommended minimum values obtained from *A Policy on Geometric Design of Highways and Streets*, 7th Edition; American Association of State Highway and Transportation Officials (AASHTO); 2018; and based on 30 mph approach speed.

^bValues shown are the intersection sight distance for a vehicle turning right or left exiting a roadway under STOP control such that motorists approaching the intersection on the major street should not need to adjust their travel speed to less than 70 percent of their initial approach speed.

As can be seen in Table 11, the available lines of sight at the Project site driveway intersection with Washington Street were found to exceed the recommended minimum sight distances for the intersection to function in a safe manner (SSD) with consideration of a three-stage exit maneuver, which is common in downtown settings with on-street parking, and based on a 30 mph approach speed along Washington Street, which is consistent with the statutory speed limit (30 mph) and slightly above the measured 85th percentile vehicle travel speed (24/26 mph) in the vicinity of the Project site.

The three stage exit maneuver is as follows: Stage 1 – the exiting motorist stops before entering the sidewalk area to observe approaching pedestrians; Stage 2 – after verifying that the sidewalk is clear, the motorist positions their vehicle across the sidewalk and into the area that is defined by the parking lane to observe approaching bicyclists and motor vehicles; and Stage 3 – the motorist exits the driveway when there is an acceptable gap in traffic. A review of the Project site driveway and the Site Plan for the Project indicates that there are clear sight lines provided to and from the sidewalk area along Washington Street to allow for an exiting motorist to complete the three-stage exit maneuver.

CONCLUSIONS AND RECOMMENDATIONS

CONCLUSIONS

VAI has conducted a TIA in order to determine the potential impacts on the transportation infrastructure associated with the proposed construction of a multifamily residential development to be located at 592 Washington Street (Route 16) in Wellesley, Massachusetts. The following specific areas have been evaluated as they relate to the Project: i) access requirements; ii) potential off-site improvements; and iii) safety considerations; under existing and future conditions, both with and without the Project. Based on this assessment, we have concluded the following with respect to the Project:

1. Using trip-generation statistics published by the ITE¹⁴ and without adjustment to account for the use of alternative modes of transportation to Single-Occupant Vehicles (SOVs), the Project is expected to generate approximately 228 vehicle trips on an average weekday, with 20 vehicle trips expected during the weekday morning peak-hour and 16 vehicle trips expected during the weekday evening peak hour;
2. The Project will not result in a significant impact (increase) on motorist delays or vehicle queuing over Existing or anticipated future conditions without the Project (No-Build conditions), with no (0) changes in LOS shown to occur as a result of the addition of Project-related traffic and Project-related impacts generally defined as an increase in average motorist delay that resulted in a corresponding increase in vehicle queuing of up to one (1) vehicle;
3. Motorists exiting the Project site driveway to Washington Street were shown to operate at LOS B during both peak hours with negligible vehicle queuing. All movements along Washington Street approaching the driveway are predicted to operate at LOS A, also with negligible vehicle queuing.
4. Independent of the Project, the Washington Street/Central Street/Grove Street intersection was identified to have a motor vehicle crash rate that was above the MassDOT District 6 average crash rate but that was below the statewide average crash rate for similar intersections. As such, specific recommendations have been provided to enhance safety at the intersection for consideration independent of the Project (see *Recommendations*); and

¹⁴Institute of Transportation Engineers, op. cit. 1.

5. Lines of sight to and from the Project site driveway intersection with Washington Street were found to exceed the recommended minimum distance for safe operation based on the appropriate approach speed and with consideration of the downtown setting within which the Project is located.

In consideration of the above, we have concluded that the Project can be accommodated within the confines of the existing transportation infrastructure in a safe and efficient manner with implementation of the recommendations that follow.

RECOMMENDATIONS

A detailed transportation improvement program has been developed that is designed to provide safe and efficient access to the Project site and address any deficiencies identified at off-site locations evaluated in conjunction with this study. The following improvements have been recommended as a part of this evaluation and, where applicable, will be completed in conjunction with the Project subject to receipt of all necessary rights, permits, and approvals.

Project Access

Access to the Project will continue to be provided by way of the existing full access driveway that intersects the east side of Washington Street approximately 120 feet south of Church Street. The following recommendations are offered with respect to the design and operation of the Project site access and internal circulation, many of which are reflected on the Site Plans.

- The full access Project site driveway will be a minimum of 24-feet in width and designed to accommodate the turning and maneuvering requirements of the largest anticipated responding emergency vehicle.
- Where perpendicular parking is proposed, the drive aisle behind the parking will be a minimum of 23 feet in order to facilitate parking maneuvers.
- Vehicles exiting the Project site to Washington Street will be placed under STOP-sign control with a marked STOP-line provided.
- All signs and pavement markings to be installed within the Project site will conform to the applicable standards of the *Manual on Uniform Traffic Control Devices* (MUTCD).¹⁵
- A sidewalk has been provided within the Project site that extends to the existing sidewalk along Washington Street. Crosswalks are provided to the north of the Project site at the Washington Street/Church Street intersection for crossing Washington Street and Church Street.
- The Project site driveway is and will continue to be a pan-type drive with the sidewalk flush across the driveway. ADA compliant wheelchair ramps will be provided for any new crosswalks that are constructed as a part of the Project.
- Electric vehicle (EV) charging stations will be installed within the Project site, with a minimum of 20% of the parking spaces to be EV ready.

¹⁵Federal Highway Administration, op. cit. 2.

- Signs, landscaping and other features that are to be installed as a part of the Project within the intersection sight triangle areas will be designed and maintained so as not to restrict lines of sight.
- Snow accumulations (windrows) within the sight triangle areas will be promptly removed where such accumulations would impede sight lines.

Off-Site

Washington Street at Central Street and Grove Street

Independent of the Project, the Washington Street/Central Street/Grove Street intersection was found to have a motor vehicle crash rate that was above the MassDOT District average crash rate but was found to be below the statewide average crash rate. Additionally, overall intersection operations, as well as specific movements from the Washington Street westbound and Grove Street northwestbound approaches are currently operating at or over capacity during the peak periods. Independent of the Project, it is recommended that an optimal traffic signal timing and phasing plan be implemented for the intersection to include: i) a review of the “yellow” and “all red” clearance intervals; and ii) consideration of restricting left-turn movements from the Washington Street westbound approach to the Grove Street southeast leg. These left-turn movements are currently permitted across two (2) northbound lanes of traffic from Washington Street which have a protected phase (“green” right-turn arrow display) when motorists are allowed to turn left onto Grove Street.

Transportation Demand Management

Regularly scheduled public transportation services are not currently provided to the Project site. To the north of the Project site, the MBTA provides Commuter Rail service on the Worcester/Framingham Line between Union Station in Worcester and South Station in Boston, with a stop at Wellesley Square Station, which is an approximate 5-minute walking distance of the Project site. In addition, the MWRTA operates the Catch Connect service within the Town of Wellesley, which is an on-demand, curb-to-curb, microtransit service. The service is booked through the MWRTA CATCH app or by phone. The MBTA also operates The Ride paratransit services for eligible persons who cannot use fixed-route transit all or some of the time due to a physical, cognitive, or mental disability in accordance with ADA requirements.

In an effort to encourage the use of alternative modes of transportation to single-occupant vehicles, the following Transportation Demand Management (TDM) measures will be implemented as a part of the Project:

- A transportation coordinator will be assigned for the Project to coordinate the TDM program;
- Information regarding public transportation services, maps, schedules, and fare information will be posted in a central location and/or otherwise made available to residents;
- A “welcome packet” will be provided to new residents detailing available public transportation services, bicycle and walking alternatives, and other commuting options;
- Amenities will be provided to support telecommuting by residents of the Project that may include collaboration space or a business office;

- Pedestrian accommodations have been incorporated within the Project and consist of a walkway that connects to the existing sidewalk along Washington Street;
- A central maildrop and package delivery station will be provided within the building; and
- Secure bicycle parking will be provided for residents that will include weather protected bicycle parking within the parking garage and exterior bicycle parking proximate to the primary building entrance.

With implementation of the aforementioned recommendations, safe and efficient access will continue to be provided to the Project site and the Project can be accommodated within the confines of the existing and improved transportation system.

APPENDIX

PROJECT SITE PLAN

AUTOMATIC TRAFFIC RECORDER COUNT DATA

TURNING MOVEMENT COUNT DATA

SEASONAL ADJUSTMENT DATA

PUBLIC TRANSPORTATION SCHEDULES

VEHICLE TRAVEL SPEED DATA

MASSDOT CRASH DATA

MASSDOT CRASH RATE WORKSHEETS AND HIGH CRASH LOCATION MAP

GENERAL BACKGROUND TRAFFIC GROWTH

TRIP-GENERATION CALCULATIONS

TRIP DISTRIBUTION DATA

CAPACITY ANALYSIS WORKSHEETS

PROJECT SITE PLAN

Proposed
Multi Family
Use

592
Washington St.
Wellesley, MA

OWNER

architects
mckay

35 Bryant Street
Dedham, MA 02026
ph:781.326.5400
www.mckayarchitects.net

WASHINGTON STREET

592 WASHINGTON STREET
PROPOSED MULTI-FAMILY BUILDING
3 STORIES - 19 UNITS

PROPOSED 6' WROUGHT IRON
FENCE TO BE TIED INTO EXISTING
FENCE.

PROPOSED 6' WROUGHT IRON
FENCE WITH GATE TO BE TIED INTO
EXISTING FENCE.

UNIT SUMMARY

UNIT 101	2BR	1718 SF
UNIT 102	2BR	1490 SF
UNIT 103	2BR	1490 SF
UNIT 104	2BR	1435 SF
UNIT 105	3BR	1887 SF
UNIT 106	3BR	1704 SF
UNIT 201	2BR	1718 SF
UNIT 202	2BR	1490 SF
UNIT 203	2BR	1490 SF
UNIT 204	2BR	1435 SF
UNIT 205	3BR	1887 SF
UNIT 206	3BR	1704 SF
UNIT 207	3BR	2277 SF
UNIT 208	2BR	1926 SF
UNIT 301	2BR	1473 SF
UNIT 302	1BR	977 SF
UNIT 303	2BR	1365 SF
UNIT 304	2BR	1376 SF
UNIT 305	3BR	1687 SF

ZONING SUMMARY

LOT SIZE	35861 SF
ZONING DISTRICT	WELLESLEY SQUARE COMMERCIAL DISTRICT
MINIMUM AREA:	NA
MINIMUM FRONTAGE:	NA
FRONT SETBACK:	5 FEET
SIDE SETBACK:	NA
REAR SETBACK:	NA
MAXIMUM HEIGHT:	45 FEET / 3 STORIES
PROPOSED HEIGHT:	40 FEET / 3 STORIES
ACCESSORY NON	
RESIDENTIAL FLOOR AREA:	5,650 SF
PARKING:	19 SPACES
	36 SPACES
4 INCLUSIONARY UNITS PROVIDED (21%)	

PROJECT SUMMARY

38,670 SQUARE FEET	
PARKING	
BIKE PARKING	
19 RESIDENTIAL UNITS	
01 ONE BEDROOM	
12 TWO BEDROOM	
06 THREE BEDROOM	
36 SPACES	
14 SPACES	
TOTAL PARKING SPACES	-36
STRUCTURED BIKE PARKING	-14

Parking Summary

OFF-STREET CAR PARKING

ACCESSIBLE SPACES

- 2

RESIDENCE SPACES

- 34

TOTAL PARKING SPACES

-36

STRUCTURED BIKE PARKING

- 14

REV #	DATE	ISSUANCE

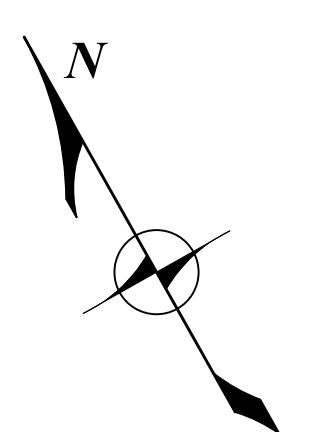
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Architects.

Comments, changes, all information and
dimensions in the field are the sole
responsibility of the contractor.

Contractor is to notify McKay
Architects of any discrepancies.

Architectural Site Plan

JOB NO	L-1.1
DATE	09.19.2025
DWG BY	RJM
CKD BY	MLM
SCALE	1" = 10'



Architectural Site Plan

L-1.1 1" = 10'

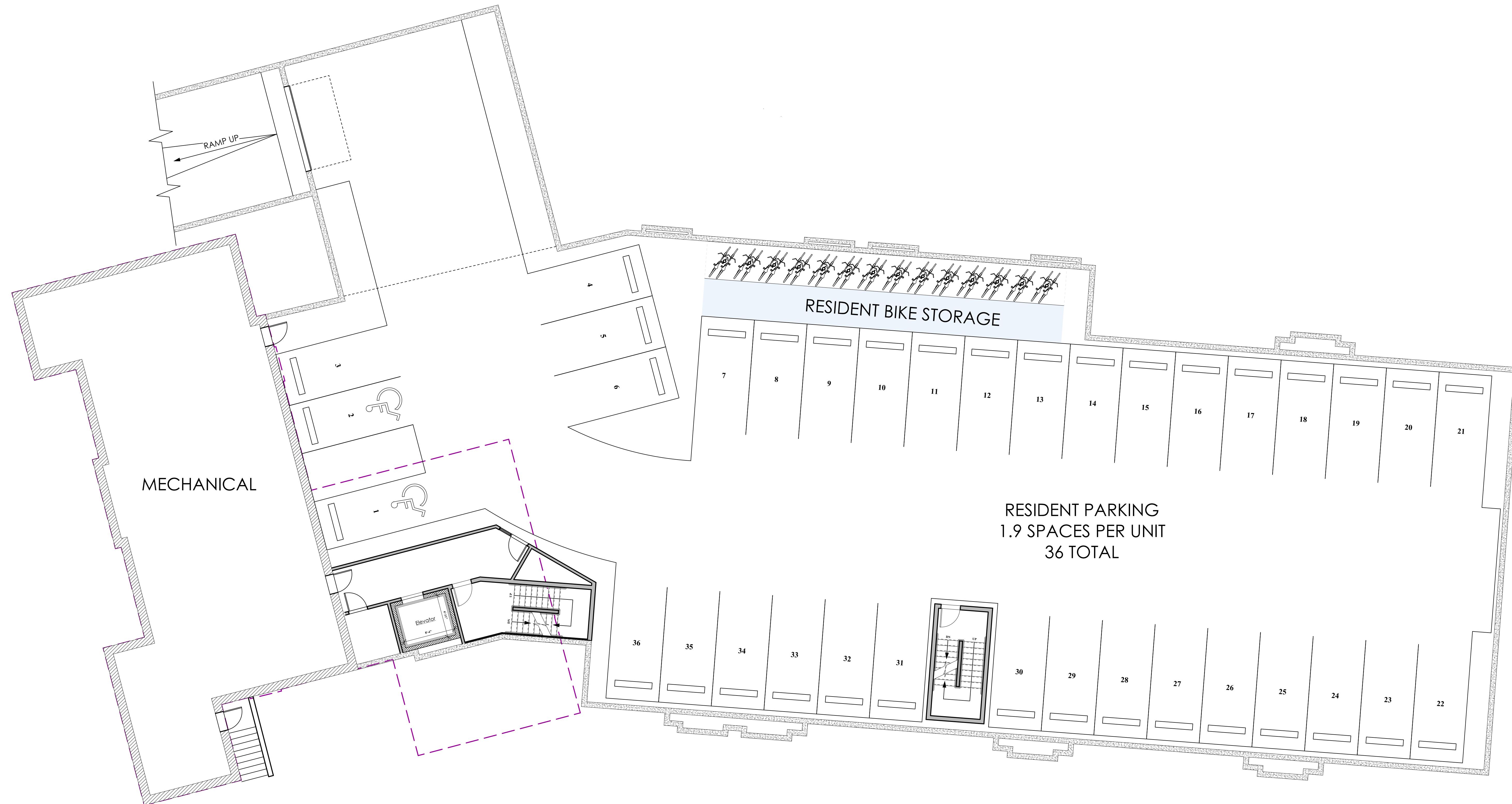
0 40 80 120



Proposed Multi Family Use

592
Washington St.
Wellesley, MA

OWNER



1 | Garage Floor Plan

A-1.1 1/8" = 1'-0"

architects
mckay

35 Bryant Street
Dedham, MA 02026

ph:781.326.5400
www.mckayarchitects.net

OWNERSHIP & USE OF DOCUMENTS

These drawings and specifications are the property and copyright of McKay Architects and shall not be used in whole or in part, or shall be assigned to a third party without the express written permission of McKay Architects.

Contractor to verify all information and dimensions in the field prior to start of construction and is to notify McKay Architects of any discrepancies

Garage Floor Plan

JOB NO	
DATE	09.19.2025
WG BY	RJM
KD BY	MLM
CALE	
1/8" = 1'-0"	



January 23, 2026

Meghan C. Jop, AICP
Executive Director
Town of Wellesley
525 Washington Street
Wellesley, MA 02482

Re: Transportation Peer Review - Proposed Multifamily Residential Development - 592 Washington Street (Route 16) Wellesley, Massachusetts

Dear Meghan:

On behalf of the Town of Wellesley, Tighe & Bond has continued our Traffic Peer Review for the proposed residential development to be located at 592 Washington Street in Wellesley, Massachusetts. The Project involves the renovation and expansion of an existing commercial building to accommodate 19 multifamily residential units.

Access to the project site will be provided by the existing driveway on Washington Street. On-site parking is proposed for 36 vehicles.

Our initial comments were summarized in a letter dated January 14, 2026. VAI has developed responses to our comments in a submission dated January 20, 2026, and provided a Traffic Impact Assessment dated November 2025. This letter provides an update on the issues we raised as well as potential new issues.

Tighe & Bond has reviewed the following documents as part of the traffic peer review:

- **Transportation Impact Evaluation (TIE);** prepared by Vanasse and Associates, Inc. (VAI); dated November 24, 2025.
- **Site Plan Set (11 sheets);** prepared by McKay Architects, dated September 15, 2025.
- **Transportation Impact Assessment (TIA);** prepared by Vanasse and Associates, Inc. (VAI); dated November 2025.
- **Response to Transportation Peer Review;** prepared by Vanasse and Associates, Inc. (VAI); dated January 20, 2026.

Our original review found that the submitted TIE was not sufficient to provide an adequate evaluation of the impact of the project. However, the supplemented material provides adequate study to identify project impacts. Due to the projected trip generation of the project, no offsite intersections are expected to meet the threshold to be considered a PSI impacted roadway.

We do have comments and suggestions related to the impact of the project on the surrounding transportation network.

For brevity's sake, we have only repeated comments where we found issues or required more information. Our original comment is in plain text, followed by VAI's response in italics. Our latest update or new comments are shown in bold font.

Study Area

Original Tighe & Bond Comment #1: The study area is not sufficient. At a minimum, the operation at the proposed site driveway is necessary to evaluate the traffic impacts of the proposed development. Due to the anticipated level of traffic to be generated by the project, study of offsite intersections and roadways are not necessary.

VAI Response: The November 2025 TIA includes an assessment of traffic volumes and operating conditions at the Project site driveway intersection with Washington Street, as well as at the following off-site intersections:

- Washington Street at Grove Street and Central Street
- Washington Street at Church Street
- Washington Street at Weston Road and Denton Road

Updated Tighe & Bond Response: The study area is sufficient to evaluate the potential impact of the project based on the expected trip distribution pattern for the Project.

Crash Data

Original Tighe & Bond Comment #2: Please review crash records for any crashes in the vicinity of the site driveway.

VAI Response: An assessment of motor vehicle crashes at the Project site driveway intersection with Washington Street and at the off-site study area intersections is provided in the November 2025 TIA. As detailed therein, no (0) motor vehicle crashes were reported to have occurred at or in the immediate vicinity of the Project site driveway intersection with Washington Street over the five-year review period 2018-2022.

Updated Tighe & Bond Response: We have reviewed the crash analysis. As noted, there were no crashes at the project site drive. The intersection of Washington Street/Central Street/Grove Street intersection was found to have a significant motor vehicle crash rate that was above the MassDOT District average crash rate.

Project-Distribution

Original Tighe & Bond Comment #3; An abbreviated trip distribution should be provided to determine the turning volumes at the site driveway.

VAI Response: The November 2025 TIA includes a trip distribution pattern for Project-generated trips developed based on a review of Journey-to-Work data obtained from the U.S. Census for the Town of Wellesley and refined using existing traffic patterns within the study area.

Updated Tighe & Bond Response: We have reviewed the trip distribution and find it to be acceptable. No further response is required.

Traffic Volume Increase

VAI provided a table showing the project generated traffic and the percentage increase on segments of roadway. We have found that it can be helpful when identifying impacted roadway segments. We have found that calculating the impact by intersection can also be helpful. Table 6 of the TIA presents the project traffic volume increase by intersection.

If the analysis were done by intersection rather than roadway segment, it would show that all of the intersections have fewer than 15 additional trips during either of the peak hours. The highest increases were at the intersection of Washington Street at Church Square, with 14 new vehicle trips in the morning peak hour

and 12 new vehicle trips in the evening peak hour. This would result in increases at the intersection of 1.7% to 1.4%.

Roadway Segment	Table 1 - Project Traffic Volume Increases					
	No Build Volumes		Project Generated Traffic		Percentage Increase	
	AM Peak Hour	PM Peak Hour	AM Peak Hour	PM Peak Hour	AM Peak Hour	PM Peak Hour
Washington Street at Grove Street and Central Street	2,148	1,932	13	11	0.6%	0.6%
Washington Street at Church Street	835	876	14	12	1.7%	1.4%
Washington Street at Weston Road and Denton Road	1,432	1,397	6	4	0.4%	0.3%

Traffic Operations Analysis

Original Tighe & Bond Comment #4: Please provide peak hour capacity analysis of the proposed site driveway's intersection with Washington Street.

VAI Response: A detailed traffic operations analysis is presented in the November 2025 TIA for the Project site driveway intersection with Washington Street and at the off-site study area intersections. With respect to the Project site driveway, All movements exiting the Project site driveway to Washington Street are predicted to operate at level-of-service(LOS) B during both peak hours with negligible vehicle queuing predicted. All movements along Washington Street approaching the driveway are predicted to operate at LOS A, also with negligible vehicle queuing. Actual operating conditions at this intersection are directly related to vehicle queuing along the Washington Street northbound approach to the Washington Street/ Central Street/Grove Street intersection during the peak periods.

Updated Tighe & Bond Response: We concur that the proposed project will not have a significant impact on operations at any of the off-site intersections. In addition, the analysis shows that the operations at the site driveway will be adequate with no significant delays.

Site Distance

Original Tighe & Bond Comment #5: Please provide a sight distance analysis at the existing/proposed site driveway. A plan should be provided that graphically shows the intersection sight distance, stopping sight distance and restrictions at the driveway.

VAI Response: A review of lines of sight at the Project site driveway intersection with Washington Street is presented in the November 2025 TIA. Based on this review, it was determined that the available lines of sight exceed the recommended minimum sight distance for the intersection to function in a safe manner (SSD) with consideration of a three-stage exit maneuver, which is common in downtown settings with on-street parking, and based on a 30 mile per hour (mph) approach speed along Washington Street, which is consistent with the statutory speed limit (30 mph) and slightly above the measured 85th percentile vehicle travel speed (24/26 mph) in the vicinity of the Project site.

The three stage exit maneuver is as follows: Stage 1 - the exiting motorist stops before entering the sidewalk area to observe approaching pedestrians; Stage 2 - after verifying that the sidewalk is clear, the motorist positions their vehicle across the sidewalk and into the area that is defined by the parking lane to observe

approaching bicyclists and motor vehicles; and Stage 3 - the motorist exits the driveway when there is an acceptable gap in traffic. A review of the Project site driveway and the Site Plan for the Project indicates that there are clear sight lines provided to and from the sidewalk area along Washington Street to allow for an exiting motorist to complete the three-stage exit maneuver.

Updated Tighe & Bond Response: As discussed in the TIA, a three stage exit maneuver will be needed to see past on-street parking. However, it is not made clear just how far a driver must pull up into the street in order to have clear sight lines. A plan should be provided that graphically shows the intersection sight distance, stopping sight distance and restrictions at the driveway.

The proposed project does not seem to provide an area for pick up/drop-off, which could be used for delivery vehicles (amazon, UPS), food delivery, or rideshare. If there is no dedicated space, drivers may either double park or stop adjacent to the driveway, which will significantly impact sight lines. Please provide an area for these uses.

Pedestrian and Bicycle Accommodations

Original Tighe & Bond Comment #6: Provide an evaluation of the pedestrian infrastructure between the site and the Wellesley Square MBTA Station. Please identify any substandard pathway links and identify what locations included Apex style ramps.

VAI Response: An evaluation of pedestrian and bicycle accommodations and access to public transportation services is presented in the November 2025 TIA. With specific regard to pedestrian accommodations at and in the vicinity of the Project site, sidewalks are generally provided along one or both sides of the study area roadways, with marked crosswalks provided for crossing one or more legs of the study area intersections.

The crossings at the Washington Street/Central Street/Grove Street intersection are included as a part of the traffic signal system at the intersection (pedestrian pushbuttons, signal indications and phasing are provided for the crossings). A pedestrian actuated Rectangular Rapid Flashing Beacon (RRFB) is provided for crossing the Washington Street south leg of Washington Street/Church Street intersection.

An inventory of sidewalk conditions along Washington Street within 1,000 feet of the Project site indicates that the sidewalks are in generally good condition. Wheelchair ramps are provided for the crossings at the study area intersections; however, many do not include tactile mats as required under the Americans with Disabilities Act (ADA) and several crossings include apex-type ramps that serve more than one crossing, which are also not ADA compliant. The sidewalk along the Project frontage is in good condition and is flush across the driveway.

Updated Tighe & Bond Response: It can be reasonably assumed that a portion of the residents of the proposed development would use public transit. Due to the location, the Wellesley Square commuter rail station is most likely. The pedestrian accommodation between Washington Street and the train platform is not adequate and should be reviewed and improved.

Recommended Off-Site Improvements

Washington Street at Central Street and Grove Street

Independent of the Project, the Washington Street/Central Street/Grove Street intersection was found to have a motor vehicle crash rate that was above the MassDOT District average crash rate but was found to be below the statewide average crash rate. Additionally, overall intersection operations, as well as specific movements from the Washington Street westbound and Grove Street northwestbound approaches are currently operating at or over capacity (i.e., LOS "E" or "F," respectively) during the peak periods. Independent of the Project, it is recommended that an optimal traffic signal timing and phasing plan be implemented for the intersection to include: i) a review of the "yellow" and "all red" clearance intervals; and ii) consideration of restricting left-turn movements from the Washington Street westbound approach to the Grove Street southeast leg. These left-turn movements are currently permitted across two (2) northbound lanes of traffic from Washington Street which have a protected phase ("green" right-turn arrow display) when motorists are allowed to turn left onto Grove Street.

Tighe & Bond Response: As mentioned in the TIA, the Washington Street northbound right turn arrow signal indication should only be used if there are no conflicts. The left turn to Grove Street does conflict. However, any turn restrictions are likely out of the scope of the proposed project. This conflict should be resolved as part of the upcoming Washington Square Improvement Project.

Site Plan Review

We have the following comments on the proposed site plans.

7. Drivers exiting parking space 30 (incorrectly labeled as #22) will not be able to see vehicles entering due to the stairwell. This space is also shorter than the rest of the parking spaces. Evaluate potential modifications to this space.
8. Parking spaces 21 and 22 will be difficult to access due to the close proximity of the far wall.
9. Please remove the wheel stops. They are unnecessary, effectively shorten the parking spaces and present a tripping hazard for pedestrians.
10. Please provide a passenger vehicle turning template showing vehicles entering the garage to verify that vehicles can access the parking area.
11. Will trash trucks access the site? The plans do not show a dumpster.
12. Please confirm that the internal doorway to the elevator is fully accessible with no vertical obstructions from the accessible spaces shown.
13. The proposed project does not seem to provide an area for pick up/drop-off, which could be used for delivery vehicles (amazon, UPS), food delivery, or rideshare. If there is no dedicated space, drivers may either double park or stop adjacent to the driveway, which will significantly impact sight lines. Please provide an area for these uses.

Conclusions & Recommendations

Tighe & Bond has reviewed the TIA's conclusions and generally agrees that the project will not have a significant impact on traffic capacity in the area. Our outstanding comments and concerns are as follows.

- **A plan should be provided that graphically shows the intersection sight distance, stopping sight distance and restrictions at the driveway.**
- **The pedestrian accommodation between Washington Street and the train platform is not adequate and should be reviewed and improved.**
- **The proposed project does not seem to provide an area for pick up/drop-off, which could be used for delivery vehicles (amazon, UPS), food delivery, or rideshare. If there is no dedicated space, drivers may either double park or stop adjacent to the driveway, which will significantly impact sight lines. Please provide an area for these uses.**
- **Drivers exiting parking space 30 (incorrectly labeled as #22) will not be able to see vehicles entering due to the stairwell. This space is also shorter than the rest of the parking spaces. Evaluate potential modifications to this space.**
- **Parking spaces 21 and 22 will be difficult to access due to the close proximity of the far wall.**
- **Please remove the wheel stops. They are unnecessary, effectively shorten the parking spaces and present a tripping hazard for pedestrians.**
- **Please provide a passenger vehicle turning template showing vehicles entering the garage to verify that vehicles can access the parking area.**
- **Will trash trucks access the site? The plans do not show a dumpster.**
- **Please confirm that the internal doorway to the elevator is fully accessible with no vertical obstructions from the accessible spaces shown.**

We appreciate the opportunity to assist the Town of Wellesley in their review of this project. If you have any questions or require additional information, please feel free to contact me directly at any time.

Very truly yours,



Alan T. Cloutier, P.E. PTOE
SENIOR ENGINEER

Copy:

Job # 2125-020-008

Wellesley 592 Washington Peer Review Response 1-23-2026.docx

Ref: 10509

January 20, 2026

Ms. Meghan C. Jop, AICP
Executive Director
Town of Wellesley
525 Washington Street
Wellesley, MA 02482

Re: Response to Transportation Peer Review
Proposed Multifamily Residential Development – 592 Washington Street
Wellesley, Massachusetts

Dear Ms. Jop:

Vanasse & Associates, Inc. (VAI) is providing responses to the comments that were identified in the January 14, 2026 *Transportation Peer Review* letter prepared by Tighe & Bond (T&B) concerning their review of the November 24, 2024 *Transportation Impact Evaluation* (the “November 2024 TIE”) that was prepared by VAI in support of the proposed multifamily residential development to be located at 592 Washington Street in Wellesley, Massachusetts (hereafter referred to as the “Project”). Listed below are the comments that were identified by T&B in the subject letter pertaining to the November 2024 TIE followed by our response on behalf of the Project proponent. We note that the Applicant filed a comprehensive Transportation Impact Assessment for the Project in November 2025 (the “November 2025 TIA”) that addresses the comments that were identified in the subject letter, a copy of which was emailed to T&B on January 16, 2026.

Study Area

Comment 1: *The study area is not sufficient. At a minimum, the operation at the proposed site driveway is necessary to evaluate the traffic impacts of the proposed development. Due to the anticipated level of traffic to be generated by the project, study of offsite intersections and roadways are area is not necessary.*

Response: **The November 2025 TIA includes an assessment of traffic volumes and operating conditions at the Project site driveway intersection with Washington Street, as well as at the following off-site intersections:**

- **Washington Street at Grove Street and Central Street**
- **Washington Street at Church Street**
- **Washington Street at Weston Road and Denton Road**

Crash Data

Comment 2: Please review crash records for any crashes in the vicinity of the site driveway.

Response: **An assessment of motor vehicle crashes at the Project site driveway intersection with Washington Street and at the off-site study area intersections is provided in the November 2025 TIA. As detailed therein, no (0) motor vehicle crashes were reported to have occurred at or in the immediate vicinity of the Project site driveway intersection with Washington Street over the five-year review period 2018-2022.**

Project-Generated Traffic

Comment: We concur with the calculation of trip generation traffic volumes.

Response: **No Response Required.**

Trip Distribution

Comment 3: An abbreviated trip distribution should be provided to determine the turning volumes at the site driveway.

Response: **The November 2025 TIA includes a trip distribution pattern for Project-generated trips developed based on a review of Journey-to-Work data obtained from the U.S. Census for the Town of Wellesley and refined using existing traffic patterns within the study area.**

Traffic Operations Analysis

Comment 4: Please provide peak hour capacity analysis of the proposed site driveway's intersection with Washington Street.

Response: **A detailed traffic operations analysis is presented in the November 2025 TIA for the Project site driveway intersection with Washington Street and at the off-site study area intersections. With respect to the Project site driveway, All movements exiting the Project site driveway to Washington Street are predicted to operate at level-of-service (LOS) B during both peak hours with negligible vehicle queuing predicted. All movements along Washington Street approaching the driveway are predicted to operate at LOS A, also with negligible vehicle queuing. Actual operating conditions at this intersection are directly related to vehicle queuing along the Washington Street northbound approach to the Washington Street/Central Street/Grove Street intersection during the peak periods.**

Sight Distance Assessment

Comment 5: Please provide a sight distance analysis at the existing/proposed site driveway. A plan should be provided that graphically shows the intersection sight distance, stopping sight distance and restrictions at the driveway.



Response: A review of lines of sight at the Project site driveway intersection with Washington Street is presented in the November 2025 TIA. Based on this review, it was determined that the available lines of sight exceed the recommended minimum sight distance for the intersection to function in a safe manner (SSD) with consideration of a three-stage exit maneuver, which is common in downtown settings with on-street parking, and based on a 30 mile per hour (mph) approach speed along Washington Street, which is consistent with the statutory speed limit (30 mph) and slightly above the measured 85th percentile vehicle travel speed (24/26 mph) in the vicinity of the Project site.

The three stage exit maneuver is as follows: Stage 1 – the exiting motorist stops before entering the sidewalk area to observe approaching pedestrians; Stage 2 – after verifying that the sidewalk is clear, the motorist positions their vehicle across the sidewalk and into the area that is defined by the parking lane to observe approaching bicyclists and motor vehicles; and Stage 3 – the motorist exits the driveway when there is an acceptable gap in traffic. A review of the Project site driveway and the Site Plan for the Project indicates that there are clear sight lines provided to and from the sidewalk area along Washington Street to allow for an exiting motorist to complete the three-stage exit maneuver.

Pedestrian and Bicycle Accommodations

Comment 6: Provide an evaluation of the pedestrian infrastructure between the site and the Wellesley Square MBTA Station. Please identify any substandard pathway links and identify what locations included Apex style ramps.

Response: An evaluation of pedestrian and bicycle accommodations and access to public transportation services is presented in the November 2025 TIA. With specific regard to pedestrian accommodations at and in the vicinity of the Project site, sidewalks are generally provided along one or both sides of the study area roadways, with marked crosswalks provided for crossing one or more legs of the study area intersections. The crossings at the Washington Street/Central Street/Grove Street intersection are included as a part of the traffic signal system at the intersection (pedestrian pushbuttons, signal indications and phasing are provided for the crossings). A pedestrian actuated Rectangular Rapid Flashing Beacon (RRFB) is provided for crossing the Washington Street south leg of Washington Street/Church Street intersection.

An inventory of sidewalk conditions along Washington Street within 1,000 feet of the Project site indicates that the sidewalks are in generally good condition. Wheelchair ramps are provided for the crossings at the study area intersections; however, many do not include tactile mats as required under the Americans with Disabilities Act (ADA) and several crossings include apex-type ramps that serve more than one crossing, which are also not ADA compliant. The sidewalk along the Project frontage is in good condition and is flush across the driveway.

Site Plan Review

Responses to the comments pertaining to the Site Plan will be provided by another member of the Project team under separate cover.



Ms. Meghan C. Jop, AICP

January 20, 2026

Page 4 of 4

We trust that this information is responsive to the comments that were identified in the January 14, 2026 letter prepared by T&B concerning their review of the materials that have been submitted in support of the Project. Responses to the comments pertaining to the Site Plans will be provided by another member of the Project team under separate cover. If you should have any questions or would like to discuss the responses from the Project team in more detail, please feel free to contact me.

Sincerely,

VANASSE & ASSOCIATES, INC.

A handwritten signature in black ink that reads "Jeffrey S. Dirk". The signature is fluid and cursive, with "Jeffrey" on the top line and "S. Dirk" on the bottom line.

Jeffrey S. Dirk, P.E., PTOE, FITE

Managing Partner

Professional Engineer in CT, MA, ME, NH, RI, and VA

JSD/jsd



W-2125-020-08
January 14, 2026

Meghan C. Jop, AICP
Executive Director
Town of Wellesley
525 Washington Street
Wellesley, MA 02482

Re: Transportation Peer Review - Proposed Multifamily Residential Development - 592 Washington Street (Route 16) Wellesley, Massachusetts

Dear Meghan:

On behalf of the Town of Wellesley, Tighe & Bond has conducted this Traffic Peer Review for the proposed residential development to be located at 592 Washington Street in Wellesley, Massachusetts. The Project involves the renovation and expansion of an existing commercial building to accommodate 19 multifamily residential units.

Access to the project site will be provided by the existing driveway on Washington Street.

On-site parking is proposed for 36 vehicles. Tighe & Bond visited the project site on January 13, 2025, to review and observe the traffic conditions in and around the project site and to verify and compare the results presented in the evaluation to what was observed in the field.

Tighe & Bond has reviewed the following documents as part of the traffic peer review:

- **Transportation Impact Evaluation (TIE);** prepared by Vanasse and Associates, Inc. (VAI); dated November 24, 2025.
- **Site Plan Set (11 sheets);** prepared by McKay Architects, dated September 15, 2025.

Our review focused on the adequacy of the evaluation with regard to industry best practices. Please note that only a brief evaluation was submitted, not a full study that would include analyzing traffic operations and related traffic impacts. We find that the submitted TIE is not sufficient to provide an adequate evaluation of the impact of the project. Due to the projected trip generation of the project, no offsite intersections are expected to meet the threshold to be considered a PSI impacted roadway, however, analysis of the site driveway is necessary.

We do have comments and suggestions related to the impact of the project on the surrounding transportation network. We have numbered the comments where we anticipate a response from the proponent.

Study Area

The TIE did not evaluate impacts on the study area roadways.

1. **The study area is not sufficient. At a minimum, the operation at the proposed site driveway is necessary to evaluate the traffic impacts of the proposed development. Due to the anticipated level of traffic to be generated by the project, study of offsite intersections and roadways are area is not necessary.**

Crash Data

Motor vehicle crash data was not obtained.

- 2. Please review crash records for any crashes in the vicinity of the site driveway.**

Project-Generated Traffic

Weekday daily and peak hour traffic volumes were determined based on standard ITE trip generation factors, using Land Use Code (LUC) 220 Multi Family Housing (low rise) with 19 Units to develop the basic number of daily and peak hour vehicle trips to the site.

Based on the calculated volumes, the project is expected to generate 228 new automobile trips (114 entering, 114 exiting) per day, 20 trips (5 entering, 15 exiting) during the weekday morning peak hour and 16 trips (10 entering, 6 exiting) during the weekday evening peak hour.

We concur with the calculation of trip generation traffic volumes.

Project-Distribution

The proposed trips were not distributed, and no calculations were provided on what routes residents are likely to use to access the site.

- 3. An abbreviated trip distribution should be provided to determine the turning volumes at the site driveway.**

Traffic Operations Analysis

No capacity analysis was conducted. Due to the projected trip generation of the project, no offsite intersections are expected to meet the threshold to be considered a PSI impacted roadway. However, the operations of the proposed driveway should be analyzed.

- 4. Please provide peak hour capacity analysis of the proposed site driveway's intersection with Washington Street.**

Site Distance

The evaluation indicated that *"there are clear sight lines provided to and from the sidewalk area along Washington Street to allow for an exiting motorist to complete the three stage exit maneuver."* However, no sight distance measurements were recorded, and the minimum required sight distance was not identified. It should be noted that on-street parking and a building to the north of the site driveway do impact sight distance.

- 5. Please provide a sight distance analysis at the existing/proposed site driveway. A plan should be provided that graphically shows the intersection sight distance, stopping sight distance and restrictions at the driveway.**

Pedestrian and Bicycle Accommodations

The review of the Pedestrian and Bicycle Facilities should be expanded to the Wellesley Square MBTA Station. The evaluation identified that many existing accessible ramps do not include detectable warning panels and mentioned that several crossings include apex -type ramps; however, the locations having apex ramps were not identified.

6. Provide an evaluation of the pedestrian infrastructure between the site and the Wellesley Square MBTA Station. Please identify any substandard pathway links and identify what locations included Apex style ramps.

Site Plan Review

We have the following comments on the proposed site plans.

7. Drivers exiting parking space 30 (incorrectly labeled as #22) will not be able to see vehicles entering due to the stairwell. This space is also shorter than the rest of the parking spaces. Evaluate potential modifications to this space.
8. Parking spaces 21 and 22 will be difficult to access due to the close proximity of the far wall.
9. Please remove the wheel stops. They are unnecessary, effectively shorten the parking spaces and present a tripping hazard for pedestrians.
10. Please provide a passenger vehicle turning template showing vehicles entering the garage to verify that vehicles can access the parking area.
11. Will trash trucks access the site? The plans do not show a dumpster.
12. "Please confirm that the internal doorway to the elevator is fully accessible with no vertical obstructions from the accessible spaces shown.

Conclusions & Recommendations

Tighe & Bond has reviewed the TIE's conclusions and generally agrees that the project will not have a significant impact on traffic capacity in the area. However, the proponent should provide information that shows the operations and safety at the proposed driveway. Our comments and concerns are as follows.

1. The study area is not sufficient. At a minimum, the operation at the proposed site driveway is necessary to evaluate the traffic impacts of the proposed development. Due to the anticipated level of traffic to be generated by the project, study of offsite intersections and roadways are area is not necessary.
2. Please review crash records for any crashes in the vicinity of the site driveway.
3. An abbreviated trip distribution should be provided to determine the turning volumes at the site driveway.
4. Please provide a capacity analysis of the proposed site driveway's intersection with Washington Street.
5. Please provide a sight distance analysis at the existing/proposed site driveway. A plan should be provided that graphically shows the intersection sight distance, stopping sight distance and restrictions at the driveway.
6. Provide an evaluation of the pedestrian infrastructure between the site and the Wellesley Square MBTA Station. Please identify any substandard pathway links and identify what locations included Apex style ramps.
7. Drivers exiting parking space 30 (incorrectly labeled as #22) will not be able to see vehicles entering due to the stairwell. This space is also shorter than the rest of the parking spaces. Evaluate potential modifications to this space.
8. Parking spaces 21 and 22 will be difficult to access due to the close proximity of the far wall.
9. Please remove the wheel stops. They are unnecessary, effectively shorten the parking spaces and present a tripping hazard for pedestrians.
10. Please provide a passenger vehicle turning template showing vehicles entering the garage to verify that vehicles can access the parking area.
11. Will trash trucks access the site? The plans do not show a dumpster.

12. Please confirm that the internal doorway to the elevator is fully accessible with no vertical obstructions from the accessible spaces shown.

We appreciate the opportunity to assist the Town of Wellesley in their review of this project. If you have any questions or require additional information, please feel free to contact me directly at any time. Once responses to the initial comments noted above have been received and reviewed, Tighe & Bond will respond to this information as appropriate.

Very truly yours,



Alan T. Cloutier, P.E. PTOE
SENIOR ENGINEER

Copy:

Job # 1750500009

Wellesley Peer Review 12-30-2025.docx

TOWN OF WELLESLEY



MASSACHUSETTS

SELECT BOARD

TOWN HALL • 525 WASHINGTON STREET • WELLESLEY, MA 02482-5992

MARJORIE R. FREIMAN, CHAIR
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WWW.WELLESLEYMA.GOV
MEGHAN C. JOP
EXECUTIVE DIRECTOR OF GENERAL GOVERNMENT SERVICES

Date: **January 27, 2026**

To: Marc Charney, Chair
Planning Board

J. Randolph Becker, Chair
Zoning Board of Appeals

From: Marjorie R. Freiman, Chair
Select Board

RE: 572 Washington Street PSI-25-02

At its meeting on **January 27, 2025**, the Select Board voted **(0-0)** to approve the Transportation Impact Assessment (TIA) by Vanasse & Associates, Inc. dated November 2025, as professionally prepared and providing sufficient evidence that the traffic conditions resulting from the proposed construction of a 19-unit multi-family structure does meet the Town's Project of Significant Impact standards for traffic, pedestrian, and bicycle safety on the Town's infrastructure.

The proposed project renovates the existing historic structure, removes a separate building in the rear of the property, removes a rear addition and adds a three-story addition with underground parking, totaling 52,770 square feet of gross floor area. Presently, the property has two commercial buildings and a surface parking lot with stormwater management system. Vehicular access to the underground parking garage is from Washington Street. The property is bounded by Washington Street to the west, multi-family residential units to the south and east, and mixed-use property to the north.

The project has been revised to provide 38 underground parking spaces to meet the required 2 spaces per unit ratio. There is metered parking along Washington Street adjacent to the property. 28 bicycle parking spaces are proposed, also underground and protect from the weather.

The project does not meet the PSI threshold for a Roadway Impacted by Development as it will not generate more than 20 vehicles related to the project in a single direction during any single

hour. The nearest unsignalized intersection is Washington Street / Church Street. The threshold for unsignalized intersections requires peak daily trips to exceed 50 at intersections and the project does not trigger that threshold at any intersection associated with the project.

The proposed project is expected to generate approximately 228 vehicle trips on an average weekday, with 20 vehicle trips (5 entering and 15 exiting) expected during the weekday morning peak-hour and 16 vehicle trips (10 entering and 6 exiting) expected during the weekday evening peak hour.

Although the project meets the PSI Standards, several concerns must be addressed in Site Plan. The project as currently designed does not provide adequate accommodation for deliveries such as Amazon or Door Dash and as such double parking could impede traffic flow on Washington Street. The lack of emergency access to the building for both fire trucks and ambulances raises concern on the impact on service time as well as the impact of the neighborhood. The only staging area would be the driveway which would then block resident/tenant access. Additional staging is likely to happen on Washington Street, which then would impede traffic flow. The Applicant must consider additional access in front of the building.

As noted above, there is no accommodation for deliveries. No turnaround is provided and the adjacent roadway has metered parking. Double parking is prohibited and is especially impactful on a high-volume road such as Washington Street. The Applicant must provide management of the delivery vehicles so that the traffic flow is not impeded.

The Traffic Committee and Traffic Consultant provide the following comments or recommendations for transportation improvements related to the project:

- The offsite truck route to Westbound Route 9 shown on the construction plans considers a route that excludes heavy trucks. The trucking route to both Eastbound and Westbound Rt. 9 must be reviewed and approved by the Police Department.
- The proposed project, with its proximity to the commuter rail station and the Wellesley Square commercial district, is expected to produce pedestrian traffic. The Traffic Impact Evaluation notes that many pedestrian ramps are not ADA compliant. Much of this will ultimately be corrected in the Wellesley Square redesign, so pending construction timing interim measures may be needed.
- The TIA's reference to public transportation is incomplete. In addition to the Catch Connect, MWRTA provides a fixed-route that can be flagged down on Washington Street. The Route 1 runs between the Natick Mall and the Woodland MBTA "T" station. In addition, MWRTA runs a hospital shuttle available from Wellesley. The MWRTA offers para-transit rides for those qualified. This information should be provided in the tenant/owner "Welcome" packet. Before a certificate of occupancy is issued, the Transportation and Mobility Manager must review the "Welcome" packet for accuracy.

Site Plan Review – The following items are recommended for review by the Zoning Board of Appeals during Site Plan Review.

Fire Truck Access Plan:

- As currently designed, access for emergency services is deficient.

Site Plan

- The front elevation drawings do not show the proposed rear addition behind the existing building, nor does it show proposed pedestrian ramps.
- Show stop sign and stop bar on plans.
- An ADA compliant path must be provided from the secondary egress from the stairwell on the south side of the building to area of safety.
- According to the Applicant trash, recycling or food waste will be collected in the basement of the original building and then rolled out to the driveway for servicing. Where will the bins be staged so as to not impact vehicular circulation.
- It appears that access to the trash/food/recycling appears to be in the same location as the accessible van loading area. This has the potential to be problematic and may not be compatible uses.
- Tree removal and tree replacement with their caliper calculations should be clearly shown on the landscape plan. It appears that three protected trees per Wellesley's Tree Bylaw will be removed. Are these trees being compensated for on site?
- Tree protection for the trees remaining including the drip lines of trees on adjacent properties should be clearly shown the Erosion & Sediment Control Plan and Construction Management Plans.

Parking Plan

- Show location of 10 proposed EV parking spaces. It does not appear space is provided for the chargers.
- 28 Bicycle parking spaces are provided. Confirm each bicycle parking space is 2'x6' with secure racks. There appears to be no designated path to the bike racks. Access to the bicycle parking spaces is through parked cars. A five-foot wide clear access path shall be provided.
- Provide bollards in front of car parking spaces # 7-13 to protect bicycle parking.
- Provide additional space between parking spaces #30, 31 and 38 and the wall.

Attachments:

[Transportation Impact Evaluation](#) by Vanasse & Associates, Inc. dated November 24, 2025

[Transportation Impact Assessment](#) by Vanasse & Associates, Inc. dated November 2025

[Transportation Peer Review prepared by Tighe and Bond dated January 14, 2026](#)
Transportation Peer Review prepared by Tighe and Bond dated January 23, 2026

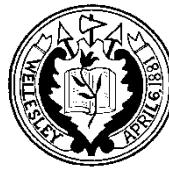
6. 7:15 Discuss and Vote Winter Supplemental to Department of Public Works

DPW has forwarded along a snow and ice supplemental request for \$500,000 following the recent weather, including substantial number of salting and de-icing events. We are anticipating significant weather this weekend, which has also factored into the total request. Following the Board's approval, the Advisory Committee will need to take this up at a subsequent meeting.

MOTION

MOVE to approve the winter supplemental request for \$500,000 from Free Cash.

TOWN OF WELLESLEY



MASSACHUSETTS

DEPARTMENT OF PUBLIC WORKS

20 MUNICIPAL WAY • WELLESLEY, MA 02481-4925

DAVID A. COHEN
DIRECTOR

TELEPHONE (781) 235-7600
FACSIMILE (781) 237-1936

January 23, 2026

Meghan Jop
Executive Director of General Government Services
Town of Wellesley
525 Washington Street
Wellesley, MA 02482

RE: FY2026 Winter Maintenance Supplemental Appropriation

Dear Meghan:

I respectfully request that you authorize a \$500,000 supplemental authorization for the FY26 Winter Maintenance Appropriation. As a result of the twelve salting events and two storm events this year, we've exhausted the original winter budget. If winter ended today, we estimate that the winter budget would have a deficit balance of approximately \$92K once all commitments are paid and salt supplies are replenished.

With the major storm predicted for this weekend and the potential of two more months of winter weather still to come, we expect that we will incur additional expenditures for snow and ice treatments during the balance of this fiscal year. In addition, this request anticipates that snow removal will be needed in the commercial districts following the storm this weekend.

For your information, we have attached a report on winter-related expenditures and a summary of all winter response events to date. As you'll see, we've had six events this year that required more than ten hours of response time and three events that occurred on a Sunday which are paid at a double time rate. The fourteen events this year have required a total of 1,800 tons of salt and 7,020 gallons of brine.

As you know, under the provisions of Massachusetts General Law Chapter 44, Section 31D, a town may incur liability and make expenditures in excess of available appropriations for snow and ice removal, provided that such expenditures are approved by the Selectmen or their designee.

The statute also requires that, in order to make use of the provisions of MGL 44:31D, the current year winter maintenance appropriation must equal or exceed the prior year's appropriation. We are in compliance with this requirement as \$379,177 was originally appropriated in FY25 and \$390,000 was appropriated in FY26.

I therefore request that you authorize the expenditure of \$500,000 in additional funds for the purpose of snow and ice removal.

Thank you for your consideration and please let me know if you have any questions or need additional information. A vote is requested and suggested motion language is included below.

Sincerely,



David A. Cohen
DPW Director

Suggested Motion: To approve the Winter Maintenance supplemental appropriation request of \$500,000 from free cash.

Cc: Madison Riley, Advisory Committee Chair
Rachel DeRoche, Finance Director
Board of Public Works

Winter Maintenance Budget
Through Payroll of Week Ending 1/20/26
and including an estimate through 1/27/26

Sources of Funds

FY26 Appropriation	\$ 390,000.00
Supplemental Authorizations Approved	\$ -
Available Funds	\$ 390,000.00

Uses of Funds

Uses of Funds - Paid to Date

Personal Services	\$ 179,795.81
Vehicle Maintenance & Fuel	\$ 98,028.08
Contractor Services	\$ 18,790.47
Salt & Sand	\$ 89,938.87
Other Supplies	\$ 10,575.78
Paid to Date	\$ 397,129.01

Uses of Funds - Services Provided/Encumbered But Not Yet Paid

Personal Services	\$ 35,000.00
Vehicle Maintenance & Fuel	\$ -
Contractor Services	\$ -
Salt & Sand	\$ 50,000.00
Other Supplies	\$ 50K to fill salt shed after 1/25 snow storm.
Not Yet Invoiced	\$ 85,000.00

Total Uses - Committed to Date	\$ 482,129.01
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Estimated Available Balance Remaining \$ (92,129.01)

Requested Additional Funds	\$ 500,000.00
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Projected Revised Balance \$ 407,870.99

Storm #	Town of Wellesley DPW FY2026 Winter Response Events As of 1/23/26											
	Response			Response Type			Accum. (Inches)	Storm Duration (Hours)	Response Duration (Hours)	Conditions	Salt Used (Tons)	Brine Used (Gal)
				Treat Only	Plow & Treat	Remove						
W01	1	Tuesday	12/02/25	X			0.02	24	20	light rain/mix	113	
	2	Sunday	12/14/25	X			0.03	11	10	snow/sleet	172	1,300
	3	Friday	12/19/25	X			0.05	12	3	sleet/ice	38	-
	4	Tuesday	12/23/25	X			1.00	18	18	sleet/snow	164	2,600
	5	Saturday	12/27/25		X		4.25	13	13	snow	255	3,120
	6	Monday	12/29/25	X			0.20	18	3	rain/freeze	70	
	7	Thursday	01/01/26	X			1.00	12	11	snow	232	
	8	Sunday	01/04/26	X			0.01	3	3	snow/sleet	48	
	9	Monday	01/05/26	X			0.02	4	4	snow/sleet	51	
	10	Tuesday	01/06/26	X			0.04	3	3	rain/freeze	198	
	11	Thursday	01/08/26	X			0.01	3	3	rain/freeze	57	
	12	Saturday	01/17/26	X			0.03	2	2	rain/sleet	59	
	13	Sunday	01/18/26		X		6.00	15	10	snow	263	
	14	Thursday	01/22/26	X			0.01	7	6	snow/sleet	79	
TOTALS / COUNT			12	2	0	12.67	145	109		1,801	7,020	

7. 7:25 FY27 Budget Update

- Discuss FY27 Operational and Capital Budgets**

An update will be provided on Health Insurance where materials in advance of the WSHG Steering Committee Meeting on 2/5/26 are at approximately 10%.

Current Projections:

The total renewal increase required based on EXCLUDING GLP1 weight loss drugs is 9.98%

The total renewal increase required RETAINING access to GLP1 weight loss drugs is 15.2%

With Health Care Reductions (Estimated at present), Cash Capital Reductions anticipated (known to date - see below), the deficit is at approximately **1,280,139**.

- Vote Select Board FY27 Operational Budgets**

MOTION

MOVE to approve the Select Board budgets as follows:

Select Board – General Government, including FMD - \$21,576,041

Public Safety – \$17,985,372

- Discuss Cash Capital Reductions and Carryforward Analysis**

To date we have received the following Cash Capital reductions:

Library 10% - \$25,000 (needs to be ratified by BLT)

NRC 22% – reduced \$35,500 (needs to be ratified by NRC)

Schools 22% – \$302,975

We are working to finalize and are discussing additional cuts from DPW, COA, and IT.

Police and Fire are fairly limited in their ability to cut or defer.

WPD – Radio Repeater – passed useful life and parts are no longer supported and Cruiser Video which was purchased in 2016, implemented in 2017, and runs 24/7. This records vehicle stops and has been instrumental in the Police Department OUI prosecutions and investigations.

Fire - Communications (mutual aid coverage, special events, and communication redundancy with WPD), Air Compressor for Air Tank Fill Station (30 years old), and Replacement of 2012 vehicle.

The Finance Department is wrapping up carryforward analysis and will submit that prior to the meeting, likely on Monday.

TOWN OF WELLESLEY - TOWN MEETING APPROVED ALLOCATION OF FUNDS

<u>SOURCES OF FUNDS</u>	<u>FY26 SOURCES OF FUNDS</u>	<u>FY27 SOURCES OF FUNDS</u>	<u>CHANGE - FY26 to FY27</u>	
			\$ Change	% Change
Real Estate & Personal Property Tax				
Within the Levy Limit	170,343,395	176,301,980	5,958,585	3.50%
Outside the Levy Limit	15,143,422	13,585,947	(1,557,475)	-10.28%
Subtotal - Real Estate & Personal Property Tax	185,486,817	189,887,927	4,401,110	2.37%
From the Commonwealth				
Chapter 70 Aid	10,952,657	11,052,657	100,000	0.91%
Lottery Aid	1,612,663	1,612,663	0	0.00%
Other Aid	63,321	63,321	0	0.00%
Subtotal - From the Commonwealth	12,628,641	12,728,641	100,000	0.79%
Local Revenue				
Motor Vehicle Excise	5,800,000	6,200,000	400,000	6.90%
Licenses and Permits	3,000,000	3,400,000	400,000	13.33%
Interest Earnings	650,000	650,000	0	0.00%
RDF Revenue	740,000	740,000	0	0.00%
Fines & forfeits	320,000	320,000	0	0.00%
Meals/Hotel/Motel Tax	750,000	750,000	0	0.00%
Penalties and Interest on Taxes	250,000	250,000	0	0.00%
Rentals	185,000	185,000	0	0.00%
Pilot Payments	75,000	75,000	0	0.00%
MLP Payment In Lieu of Taxes	1,000,000	1,000,000	0	0.00%
MLP Indirect Costs	200,000	206,000	6,000	3.00%
Other Local Revenues	156,994	156,994	0	0.00%
Subtotal - Local Revenue	13,126,994	13,932,994	806,000	6.14%
Other Sources				
Water/Sewer Indirect costs	175,411	180,673	5,262	3.00%
Parking Meter Receipts	743,135	770,835	27,700	3.73%
Free Cash to balance budget	2,246,401	2,200,000	(46,401)	-2.07%
Free Cash items	6,549,525		(6,549,525)	
Free Cash Supplements FY26	2,021,735		(2,021,735)	
Free Cash Supplements FY26 - ATM26	47,244		(47,244)	
Appropriated CPA Surcharge	2,065,000		(2,065,000)	
CPA Funds applied to North 40	550,244		(550,244)	
Town Clerk reimbursed elections	0		0	0.00%
Subtotal - Other Sources	14,398,695	3,700,952	-10,697,743	-74.30%

Exhibit D

TOTAL SOURCES OF FUNDS	225,641,147	220,250,514	(5,390,633)	-2.39%
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225,593,903

47,244

Exhibit B

USES OF FUNDS	FY26 USE OF FUNDS (Tax Rate)			FY27 USE OF FUNDS (Request)			CHANGE - FY26 to FY27			
	Pers Svcs	Expenses	Total Ops	Pers Svcs	Expenses	Total Ops	Variance Pers Svcs	Variance Expenses	Variance Total - \$	Variance Total - %
GENERAL GOVERNMENT										
Select Board - Administration										
Executive Director's Office	764,068	49,000	813,068	786,922	50,450	837,372	22,854	1,450	24,304	2.99%
Climate Action Committee	173,234	10,291	183,525	176,699	10,591	187,290	3,465	300	3,765	2.05%
Central Administrative Services	0	14,600	14,600	0	15,000	15,000	0	400	400	2.74%
Finance Department	616,432	14,100	630,532	634,882	14,450	649,332	18,450	350	18,800	2.98%
Information Technology	905,437	889,600	1,795,037	928,767	960,600	1,889,367	23,330	71,000	94,330	5.26%
Treasurer & Collector	463,238	140,700	603,938	476,486	144,900	621,386	13,248	4,200	17,448	2.89%
Town Report	0	2,250	2,250	0	2,250	2,250	0	0	0	0.00%
Select Board - Human Services										
Council on Aging	548,646	88,850	637,496	561,868	91,500	653,368	13,222	2,650	15,872	2.49%
West Suburban Veterans District	0	85,264	85,264	0	89,306	89,306	0	4,042	4,042	4.74%
Youth Commission	111,927	17,090	129,017	114,259	17,090	131,349	2,332	0	2,332	1.81%
Select Board - Facilities										
Facilities Management	5,778,023	4,477,682	10,255,705	5,845,659	4,699,021	10,544,680	67,636	221,339	288,975	2.82%
Land Use Departments relocation	0	128,500	128,500	0	128,900	128,900	0	400	400	0.31%
Select Board - Other Services										
Housing Development Corporation	0	6,500	6,500	0	6,500	6,500	0	0	0	0.00%
Historical Commission	0	750	750	0	750	750	0	0	0	0.00%
Memorial Day	0	5,950	5,950	0	6,069	6,069	0	119	119	2.00%
Celebrations Committee	0	20,000	20,000	0	20,600	20,600	0	600	600	3.00%
Cultural Council	0	8,500	8,500	0	8,755	8,755	0	255	255	3.00%
Zoning Board of Appeals	96,753	9,430	106,183	100,286	10,030	110,316	3,533	600	4,133	3.89%
Select Board - Shared Services										
Law	0	480,000	480,000	0	490,000	490,000	0	10,000	10,000	2.08%
Audit Committee	0	63,000	63,000	0	75,600	75,600	0	12,600	12,600	20.00%
Risk Management	0	1,010,592	1,010,592	0	1,037,600	1,037,600	0	27,008	27,008	2.67%
Street Lighting	0	142,000	142,000	0	142,000	142,000	0	0	0	0.00%
Class and Comp Study	3,220	0	3,220	0	0	0	(3,220)	0	(3,220)	-100.00%
Contract Settlements	0	0	0	1,200,000	0	1,200,000	1,200,000	0	1,200,000	#DIV/0!
Subtotal - Select Board - General Government	9,460,978	7,664,649	17,125,627	10,825,828	8,021,962	18,847,790	1,364,850	357,313	1,722,163	10.06%

Exhibit B

USES OF FUNDS	FY26 USE OF FUNDS (Tax Rate)			FY27 USE OF FUNDS (Request)			CHANGE - FY26 to FY27			
	Pers Srvs	Expenses	Total Ops	Pers Srvs	Expenses	Total Ops	Variance Pers Srvs	Variance Expenses	Variance Total - \$	Variance Total - %
Other General Government										
Town Clerk	383,096	101,290	484,386	447,741	127,990	575,731	64,645	26,700	91,345	18.86%
Board of Assessors	374,981	129,800	504,781	383,884	141,280	525,164	8,903	11,480	20,383	4.04%
Planning Board	428,853	85,600	514,453	424,660	85,600	510,260	(4,193)	0	(4,193)	-0.81%
Advisory Committee	31,827	20,070	51,897	32,464	20,670	53,134	637	600	1,237	2.38%
Reserve Fund	0	175,000	175,000	0	175,000	175,000	0	0	0	0.00%
Human Resources Board	529,328	43,460	572,788	541,983	46,979	588,962	12,655	3,519	16,174	2.82%
HR Salary Adjustment Carryforward	-115,632	0	-115,632	0	0	0	115,632	0	115,632	-100.00%
HR Salary adjustments	26,061	0	26,061	300,000	0	300,000	273,939	0	273,939	1051.15%
Subtotal - Other General Government	1,658,514	555,220	2,213,734	2,130,732	597,519	2,728,251	472,218	42,299	514,517	10
GENERAL GOVERNMENT TOTAL	11,119,492	8,219,869	19,339,361	12,956,560	8,619,481	21,576,041	1,837,068	399,612	2,236,680	11.57%
PUBLIC SAFETY - Select Board										
Police Department	7,674,767	902,429	8,577,196	7,790,208	938,516	8,728,724	115,441	36,087	151,528	1.77%
Injured on Duty	see Risk Management Dept 945			see Risk Management Dept 945						
Special School Police	142,285	3,887	146,172	148,792	4,004	152,796	6,507	117	6,624	4.53%
Fire Department	7,581,182	513,180	8,094,362	7,776,632	528,561	8,305,193	195,450	15,381	210,831	2.60%
Building Department	711,884	39,100	750,984	740,104	39,100	779,204	28,220	0	28,220	3.76%
Sealer of Weights & Measures	16,451	2,600	19,051	16,780	2,675	19,455	329	75	404	2.12%
PUBLIC SAFETY TOTAL - Select Board	16,126,569	1,461,196	17,587,765	16,472,516	1,512,856	17,985,372	345,947	51,660	397,607	2.26%
DEPARTMENT OF PUBLIC WORKS										
Engineering	414,873	47,662	462,535	423,216	47,662	470,878	8,343	0	8,343	1.80%
Highway	1,025,157	379,580	1,404,737	1,050,256	400,580	1,450,836	25,099	21,000	46,099	3.28%
Fleet Maintenance	223,230	44,331	267,561	227,025	44,331	271,356	3,795	0	3,795	1.42%
Park	1,690,805	535,720	2,226,525	1,710,508	535,720	2,246,228	19,703	0	19,703	0.88%
Recycling & Disposal	1,387,636	1,868,361	3,255,997	1,407,525	1,943,749	3,351,274	19,889	75,388	95,277	2.93%
Management	470,480	22,865	493,345	483,345	22,865	506,210	12,865	0	12,865	2.61%
Winter Maintenance	0	390,000	390,000	0	390,000	390,000	0	0	0	0.00%
PUBLIC WORKS TOTAL	5,212,180	3,288,519	8,500,699	5,301,875	3,384,907	8,686,782	89,695	96,388	186,083	2.19%
WELLESLEY FREE LIBRARY										
Library Trustees	2,512,917	813,736	3,326,653	2,573,194	842,203	3,415,397	60,277	28,467	88,744	2.67%
LIBRARY TOTAL	2,512,917	813,736	3,326,653	2,573,194	842,203	3,415,397	60,277	28,467	88,744	2.67%
RECREATION										
Recreation Commission	485,452	25,000	510,452	496,005	25,550	521,555	10,553	550	11,103	2.18%
RECREATION TOTAL	485,452	25,000	510,452	496,005	25,550	521,555	10,553	550	11,103	2.18%

Exhibit B

<u>USES OF FUNDS</u>	<u>FY26 USE OF FUNDS (Tax Rate)</u>			<u>FY27 USE OF FUNDS (Request)</u>			<u>CHANGE - FY26 to FY27</u>			
	Pers Svcs	Expenses	Total Ops	Pers Svcs	Expenses	Total Ops	Variance Pers Svcs	Variance Expenses	Variance Total - \$	Variance Total - %
HEALTH										
Board of Health	1,109,792	185,000	1,294,792	1,138,700	191,500	1,330,200	28,908	6,500	35,408	2.73%
Mental Health Services	0	300,572	300,572	0	309,589	309,589	0	9,017	9,017	3.00%
HEALTH TOTAL	1,109,792	485,572	1,595,364	1,138,700	501,089	1,639,789	28,908	15,517	44,425	2.78%
NATURAL RESOURCES										
Natural Resources Commission	342,953	36,350	379,303	352,554	37,400	389,954	9,601	1,050	10,651	2.81%
Morses Pond Project - (NRC, DPW, Rec)	0	177,000	177,000	0	182,250	182,250	0	5,250	5,250	2.97%
NATURAL RESOURCES TOTAL	342,953	213,350	556,303	352,554	219,650	572,204	9,601	6,300	15,901	2.86%
NON-SCHOOL TOTAL	36,909,355	14,507,242	51,416,597	39,291,404	15,105,736	54,397,140	2,382,049	598,494	2,980,543	5.80%
WELLESLEY PUBLIC SCHOOLS										
Instruction	56,859,126	3,218,399	60,077,525	58,256,527	3,168,176	61,424,703	1,397,401	(50,223)	1,347,178	2.24%
Administration	1,456,475	154,875	1,611,350	1,517,740	264,812	1,782,552	61,265	109,937	171,202	10.62%
Operations	1,926,389	2,514,399	4,440,788	2,008,119	2,582,079	4,590,198	81,730	67,680	149,410	3.36%
Special Education	21,647,437	6,257,926	27,905,363	21,727,106	7,331,516	29,058,622	79,669	1,073,590	1,153,259	4.13%
SCHOOL TOTAL	81,889,427	12,145,599	94,035,026	83,509,492	13,346,583	96,856,075	1,620,065	1,200,984	2,821,049	3.00%
EMPLOYEE BENEFITS										
Group Insurance	0	25,598,232	25,598,232	0	27,626,306	27,626,306	0	2,028,074	2,028,074	7.92%
Workers Compensation	0	740,348	740,348	0	725,963	725,963	0	(14,385)	(14,385)	-1.94%
OPEB Liability Fund	0	3,100,000	3,100,000	0	3,000,000	3,000,000	0	(100,000)	(100,000)	-3.23%
Retirement Contribution	0	9,310,462	9,310,462	0	9,225,765	9,225,765	0	(84,697)	(84,697)	-0.91%
Unemployment Compensation	0	100,000	100,000	0	100,000	100,000	0	0	0	0.00%
Compensated Absences	0	130,000	130,000	0	130,000	130,000	0	0	0	0.00%
EMPLOYEE BENEFITS TOTAL	0	38,979,042	38,979,042	0	40,808,034	40,808,034	0	1,828,992	1,828,992	4.69%
ALL PERSONAL SERVICES & EXPENSES	118,798,782	65,631,883	184,430,665	122,800,896	69,260,353	192,061,249	4,002,114	3,628,470	7,630,584	4.14%

Exhibit B

USES OF FUNDS	FY26 USE OF FUNDS (Tax Rate)			FY27 USE OF FUNDS (Request)			CHANGE - FY26 to FY27			
	Pers Srvs	Expenses	Total Ops	Pers Srvs	Expenses	Total Ops	Variance Pers Srvs	Variance Expenses	Variance Total - \$	Variance Total - %
CAPITAL & DEBT										
<i>Departmental Cash Capital</i>										
Public Works Capital	0	3,892,000	3,892,000	0	3,653,000	3,653,000	0	(239,000)	(239,000)	-6.14%
School Capital	0	1,299,481	1,299,481	0	1,078,363	1,078,363	0	(221,118)	(221,118)	-17.02%
Facilities Capital	0	1,941,000	1,941,000	0	1,577,000	1,577,000	0	(364,000)	(364,000)	-18.75%
Select Board Capital	0	436,376	436,376	0	364,554	364,554	0	(71,822)	(71,822)	-16.46%
Library Capital	0	227,000	227,000	0	209,000	209,000	0	(18,000)	(18,000)	-7.93%
NRC Capital	0	320,000	320,000	0	175,000	175,000	0	(145,000)	(145,000)	-45.31%
Morses Pond	0	35,000	35,000	0	0	0	0	(35,000)	(35,000)	100.00%
Subtotal - Cash Capital	0	8,150,857	8,150,857	0	7,056,917	7,056,917	0	(1,093,940)	(1,093,940)	-13.42%
<i>Debt Service</i>										
Current Inside Levy Debt Service - Issued	0	3,568,406	3,568,406	0	5,021,617	5,021,617	0	1,453,211	1,453,211	40.72%
Outside Levy Debt Service - Issued/Unissued	0	15,693,666	15,693,666	0	14,135,391	14,135,391	0	(1,558,275)	(1,558,275)	-9.93%
Subtotal - Debt Service	0	19,262,072	19,262,072	0	19,157,008	19,157,008	0	(105,064)	(105,064)	-0.55%
CAPITAL & DEBT TOTAL		27,412,929	27,412,929	0	26,213,925	26,213,925	0	(1,199,004)	(1,199,004)	-4.37%
SPECIAL ITEMS										
<i>Receipts Reserved for Appropriation</i>										
Traffic & Parking Operations	127,235	615,900	743,135	127,235	643,600	770,835	0	27,700	27,700	3.73%
Community Preservation Appropriated	0	2,065,000	2,065,000			0	0	(2,065,000)	(2,065,000)	-100.00%
Free Cash items (2027)	0	6,549,525	6,549,525			0	0	(6,549,525)	(6,549,525)	-100.00%
Free Cash (2026)	0	2,021,735	2,021,735			0	0	(2,021,735)	(2,021,735)	-100.00%
Property Tax Abatements	0	920,944	920,944	0	950,000	950,000	0	29,056	29,056	3.16%
State & County Assessments	0	1,497,214	1,497,214	0	1,534,644	1,534,644	0	37,430	37,430	2.50%
SPECIAL ITEMS TOTAL	127,235	13,670,189	13,797,553	127,235	3,128,244	3,255,479	0	(10,542,073)	(10,542,073)	-76.41%
TOTAL USES OF FUNDS			225,641,147			221,530,653			(4,110,494)	-1.82%
TOTAL SOURCES OF FUNDS			225,641,147			220,250,514			(5,390,633)	-2.39%
SURPLUS (DEFICIT)			0			(1,280,139)				

8. **7:45 Annual Town Meeting (ATM) Preparation**
• **Article 6: Discuss and Vote Town Clerk Salary**

The Select Board discussed the Town Clerk salary at its meeting last week, during which one member suggested an alternative approach to setting the starting salary.

Original Proposal: The original proposal for setting the Town Clerk's salary was based on an average of elected official salaries from comparable communities. That averaged figure was then adjusted to include the approved cost-of-living adjustment (2%), a service adjustment (1%), and a component attributable to merit pay. While the Town Clerk is not evaluated under the same performance review process as other department heads, a portion of the salary adjustment was included to maintain internal equity, recognizing that department heads have the potential to earn merit compensation.

Alternative Approach: As an alternative, one Select Board member suggested relying on the Town's Classification and Compensation Plan completed last year. As the Town Clerk position is elected, it is not part of the Class and Comp plan, although the HR Director has confirmed the responsibilities would align the role at a Job Group 14 with a salary range of \$126,684 to \$171,023, and the Town's standard practice has been to place employees at the minimum of their assigned range. Under this approach, the recommended starting salary would be \$126,684. Applying the approved cost-of-living adjustment, service adjustment, and the portion attributable to merit pay for department heads would result in an adjusted salary of \$133,772, while maintaining consistency with the Town's adopted compensation framework.

The range in question would be the proposed \$129,689 originally recommended to \$133,773 with the current Class and Comp Range being the base for consideration.

MOTION

MOVE to approve the Town Clerk salary of _____ for Article 6 on the 2026 Annual Town Meeting Warrant.

- Article 27: Discuss and Vote Use of \$8,000 of TNC (Uber/Lyft) Funds

Transportation and Mobility Manager Sheila Page is requesting an appropriation from the annual Transportation Network Company (TNC) Annual Allocation to reduce single-occupancy vehicle trips. TNCs such as Uber and Lyft pay a \$0.20 surcharge per ride. Half of that (\$0.10) is returned to the community in which the ride originates. Wellesley's 2024 portion is \$29,941.10.

Municipal TNC funds may be used for planning, programs, and projects that support or improve transportation, including:

Transportation access and mobility

- First/last-mile connections (e.g., shuttles, microtransit pilots)
- Transit access programs for seniors, youth, or low-income residents
- Programs that reduce single-occupancy vehicle trips

Education, outreach, and safety

- Bicycle and pedestrian safety education
- Transit training and “how-to-ride” programs
- Travel behavior education (e.g., encouraging use of buses, rail, or microtransit)
- Safe Routes to School-type programming

Planning and pilot programs

- Transportation demand management (TDM) initiatives
- Mode-shift or trip-reduction pilots
- Short-term demonstration projects
- Incentive programs tied to transit use (passes, credits, challenges), when structured as pilots

Capital or one-time transportation improvements

- Bike, pedestrian, or curb-management improvements
- Technology or equipment that supports mobility programs
- Data, studies, or planning efforts related to transportation systems

The request from the Transportation and Mobility Manager is to use up to \$8,000 for these very purposes. Funds not expended would return to the TNC Fund as they are earmarked for these purposes.

Demand and uptake for the proposed education programs

- The Board asked how staff was assessing **actual demand** for programs like bike education, family riding classes, and transit training. We need to build the demand. Public transit like Catch Connect and the Rt 1 bus, the T and the commuter rail are opportunities to get people out of their cars and for our kids and seniors to gain independence and learn a life skill. Riding public transit can be intimidating, particularly alone. We need to let people know what is available and how to access it. Anecdotally - Sheila has been asked to provide more education on safe bicycling as well as how to ride the Catch Connect and Route 1. Lexington's classes have been popular.
- We can track participation.

At the previous discussion staff referenced comparisons to other communities and anecdotal demand from parents and residents, but this prompted discussion about **how demand is measured and projected**. Offering high quality classes will create a demand. We can track participation and waitlists. We can also reconnect with participants to see if the class changed their travel behavior or comfort.

Board members asked for clarification on **what the incentive programs would look like** (e.g., encouraging MWRTA use), signaling interest in more specificity on structure, targets, and effectiveness. While this portion of the discussion was brief, staff noted that the Town previously participated in the **May Mode Shift Challenge**, which was not particularly successful as a standalone effort. A potential next step would be to pair incentive programs with educational programming—for example, reintroducing a mode-shift challenge in September alongside transit or biking classes—to better support behavior change.

MOTION

MOVE to approve the request to Town Meeting to authorize \$8,000 in TNC funds.

9. 8:10 Citizens' Leadership Academy Update

Working with Marjorie Freiman and Lise Olney in early 2025, Corey conducted background research and engaged in conversations with peer communities about citizen leadership academies. After months of discussions with department heads and other staff, the Select Board Office staff are prepared to “kick-off” Wellesley’s inaugural Citizens’ Leadership Academy (WCLA) in March 2026!

Included in the FNM packet is draft of the WCLA’s work plan, which includes the “course list” and proposed dates of the course. Details of each class are still being finalized with each department, so please read the work plan as a living and evolving document. The work plan also includes notes from Chair Marjorie Freiman when she first brought the idea to the Board’s attention, notes from subsequent meetings, and a sample of the draft application that will go out to the public in February. The deadlines contained in the work plan are also subject to change.



SELECT BOARD

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WWW.WELLESLEYMA.GOV

Wellesley Citizens' Leadership Academy

Introduction

The Town of Wellesley's governmental functions depend heavily on the engagement of its citizens. With 11 elected boards and an additional 20 appointed boards, more than 200 volunteers work with Town staff daily in decision-making processes affecting a wide range of issues. Effective policymaking, and administrative and management decisions are improved by the participation of knowledgeable residents, familiar with the decentralized structure and method of decision-making; the broader context within which decisions are made; the factors to consider for each subject; familiarity with colleagues, both volunteer and staff; understanding of the Town Meeting legislative process; and cross-departmental planning and work.

A proven and effective model exists to increase citizen participation in local government – to raise awareness, increase knowledge and familiarity with municipal governing and functions, introduce citizens to senior staff in their town, deepen commitment to successful local initiatives, and expand the pipeline of residents who wish to serve their town – at very low cost and with relatively straightforward planning.

Citizens' Leadership Academies are currently being run successfully in many other municipalities and are a positive catalyst in building volunteer cohorts, producing knowledgeable citizenry, and creating and deepening community relationships.

Goals:

- **More informed residents**
- **More engaged residents – In 3 communities, 25% of local “cadets” run for office or TM, join the Town as an employee or join a board/committee. Barnstable has served 1000 graduates since 2003 and has carried a 15-person waitlist for a few years.**
- **More Town-wide volunteers**
- **Dispel misinformation about Town government**
- **Provide guidance on where to find accurate information: real-time introduction to those who make the town work and to whom questions can be addressed**
- **Departments take ownership and pride in sharing their space and their work**

Determinants of success:

- Senior staff, department heads and board/committee officers and members agree on the huge potential of the program
- Creative, engaging activities for participants – every session must have a fun and interactive component
- First-year success provides basis for subsequent showing of popularity, success, and impact
- Department heads' attendance at their staffs' session supports presenters, provides backup for challenging questions
- Early planning
 - Outreach to all potential communities and constituencies through every conceivable outlet, including public interface, telephone calls and meetings to engage leaders in recruitment
 - DH identifies staff member(s) to make presentation – provides public presentation and speaking experience and “face time” and introduces the public to new staff members
 - Presentations are given to the Assistant Executive Director 4 weeks in advance to review IT needs, goals of specific session, breakout activities/tours, and set up for success
 - Opportunities to do “dry runs” are offered to staff
 - All materials are compiled into a binder
 - Social media set up a month prior to application opening; standing webpage is created, discussed at board meetings, info shared on public access channels, in newsletters, posters, emails, school and community group distribution lists and through departments
 - Think about “backstage view” that public doesn’t typically get – hands-on activities, setting up presentation portion of session in a “work” area

Select Board collaboration in program planning

- SB office will prepare a template for weekly presentations which presenter can customize; presentations will follow a similar format for consistency
- Schedule will consider “high” season for departments (i.e. DPW not scheduled after October), and early submission of presentations will facilitate emergency changes in schedule
- Assistant Executive Director and/or SB member will attend every session, take notes, reflect on session successes and opportunities, and will contact every staff member following presentation to thank and congratulate them for participating
- SB will compile binder containing:
 - Session planner – dates, departments, IT needs, breakout activities/tours
 - All presentations
 - Budget book, Annual Town Report, Town Meeting Guide, department swag (if desired), flyers of upcoming events, weekly feedback form (allows adjustment and question response in real time), board/committee volunteer forms

Draft 2025 Academy Schedule

Date	Topic	Department/Staff	Notes
Session 1 03/26/26	Town Government Overview #1	ED/AED Finance Town Clerk Veteran Service Officer	Town government structure, Discussion of budget process, role of Finance Department in town government, Town meeting process (KC), work of Town Clerk's office “Around the World” concept at Town Hall – stations, split the class into smaller sections for small group activities/learning modules
Session 2 04/02/26	Police	Police Chief & PD staff	Held at WPD HQ Hands on learning, PD personnel walk the class through a “day in the life of a police officer” “Arrest” a resident to show the process; booking, holding, etc.; logging evidence.
Session 3 04/09/26	Sustainability	Sustainability Director & Staff	Overview of department At-home visit? More tactile approach, show residents how the work of the department is being implemented “on the ground”
Session 4 04/16/26	Affordable/Public Housing	Youth Director/Social Worker COA Senior Services Dir. Health Dept. SWs	Barton Road meeting – fundamentals of public and affordable housing; meet with residents; tour of vacant unit; hands-on demonstration of the housing crisis in MA
4/23/26	NO CLASS	<i>APRIL SCHOOL VACATION WEEK</i>	
Session 5 04/30/26	DPW	DPW Director & Staff	DPW tour? RDF tour? DPW personnel onsite for demonstrations. Swag?
Session 6 05/7/26	Fire	Fire Chief, Asst. Chief & Staff	Tour of HQ Burn Pit Touch-a-Truck, equipment demonstrations, fire extinguisher training
Session 7 05/14/26	NRC	NRC Director	Trail walk Tree planting Get outdoors!

<i>Tuesday, May 19th at 6:30pm</i>	<i>Select Board Meeting</i>		<i>Brief graduation ceremony at Select Board meeting – certificates and photos with the Board</i>
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Other potential areas to cover: role of Civil Discourse Initiative, permanent DEI standing committee, arts & culture efforts, etc. (retain focus on staff?)

Next Steps:

1. Select Board approval → January 27, 2026

- Bring proposal to the Board at an upcoming meeting
- *Do we want to offer dinner or just tell people to BYOD*
- Can we request funding from Babson fund?
 - o Printing and mailing
 - o Binders
 - o Cadet bags
 - o Pins and Certificates
 - o Either pizza and salad, or NO dinner, but provide drinks, cookies, oranges?

2. Finalize Schedule/Application → First week of February 2026

- Confirm availability with department heads and/or staff and reserve space required for the events, meetings, etc.
- Finalize the WCLA application RELEASE Early July and advertise all summer and with all back-to-school and fall information, including STM info
 - o (1 town does first-come first-served, 1 gives priority to those not currently engaged in town gov't. In 1 community, Asst TMgr reviews and recommends participants and TMgr makes final decision)

3. Advertising → First weeks of February 2026

- Publicize the WCLA to residents and start accepting applications
- Swellesley Report, News & Announcements, Facebook/Instagram, COA Newsletter, fliers at Library/COA/WHA properties/Warren Building, Wellesley Media, All board/committees, School publications, town department newsletters

4. Applications → Go out after February school vacation; deadline in early March (2 weeks)

Logistics

Information is from initial proposal to Select Board from Marjorie Freiman in January 2025

- Annual fall program (September-November) to begin 2025
- 8 weekday evening sessions from 6:00 – 8:30
- Attendance at sessions is expected (*family or emergency circumstances may require occasional absence but missing excessive classes may result in request to withdraw*)
- Sessions facilitated by town staff
- Interactive, informative classes; presentations, discussions, visits to Town departments, and hands-on activities
- Free of charge and open to anyone (17+) who lives or works in the Town of Wellesley
- Admission is through application
- Class size limited to 25 (a waiting list for the next available session will be created if applications exceed available space)
- Signature of liability waiver required
- Once accepted into the WCLA, participants will receive an orientation packet with welcome letter, details, schedule, and location of all sessions. Participants sign participant agreement with waiver and photo release form (optional)
- Class locations are accessible to all; however, tours may include areas of buildings not open to the public and therefore the entire building may not be fully accessible.
- Most meetings will include a pre-meeting questionnaire and many will include a post-meeting feedback form (what did you like, what do you wish we had included, what follow-up would you like?)
- Completion certificates (and town pins) will be awarded to participants at a “Graduation Ceremony” with the Select Board at their meeting on November 18 and group photos will be taken. Photos taken throughout the session will be shared in a photo montage.

General outline of each session (may include fewer than all of the following):

- Introductions of department/board/service/organization and participants
- Organizational structure (size of department, number of employees, reporting structure...how it fits in org chart)
- Key Responsibilities
- Budget
- Statistics (apparently everyone loves them)
- Near-future plans and opportunities – what is something people wouldn't necessarily know or something interesting about your department?
- Challenges
- Engagement Opportunities/ tour of facility
- Participant activities

Miscellaneous notes

- All school civics teachers went through Danvers academy
- Danvers also allows 1-2 employees each year but does not count them in total number – great especially for new employees

- Information on new academies shared with graduates to assist recruitment
- Barnstable did a Saturday bus tour around town to show their schools, coastlines, marinas, etc.

**TOWN OF WELLESLEY
CITIZENS' LEADERSHIP ACADEMY
APPLICATION
Spring 2026 INAUGURAL Session**

The Town of Wellesley Citizens' Leadership Academy is an 8-week series of classes and activities designed to:

- (1) give participants an up-close view and familiarize them with the services of local government; and
- (2) develop informed and civically minded future community leaders.

To be considered for the Spring 2026 class, please complete and return the application below by **5:00 p.m. on Thursday, March 12**. Notice of acceptance into the academy will be communicated to applicants by **Friday, March 20**. For more information on the academy, please visit the Town's website: www.wellesleyma.gov/citizensacademy.

Academy size is limited to 24 participants.

Requirements for Participation:

- Must be a current resident or employee of the Town of Wellesley
- Must be 18 years old by January 1, 2025
- Must be willing to sign a liability waiver (active class!)

Application:

First Name: _____ Last Name: _____

Mailing Address: _____

Length of time you have lived/worked in Wellesley _____ live work

Telephone number _____ Email: _____

Occupation: _____

Name

Signature

Please describe your past and present community/civic involvement, if any:

Please share why you are interested in participating in the Citizens' Leadership Academy:

Class dates are listed below. All sessions are from **6:30 -9:00 p.m.** Attendance at most sessions is expected. If there are dates you know you cannot attend, please check those below:

<input type="checkbox"/> Thursday, March 26	<input type="checkbox"/> Thursday, April 30
<input type="checkbox"/> Thursday, April 2	<input type="checkbox"/> Thursday, May 7
<input type="checkbox"/> Thursday, April 9	<input type="checkbox"/> Thursday, May 14
<input type="checkbox"/> Thursday, April 16	<input type="checkbox"/> Tuesday, May 19 [Graduation ceremony]

How did you hear about the Citizens' Leadership Academy?

Town website
 Town email
 Social media
 Word of mouth/Friend
 Flyer (from where?) _____
 Other – please explain _____

10. 8:30 Administrative Matters

- **Discuss and Vote Minutes**

Please find the minutes of the January 6, 2026, meeting for the Board's review and approval.

MOTION

MOVE to approve the minutes of January 6, 2026.

1 Approved:

2

3 **Select Board Meeting:** January 6, 2026

4 **Present:** Marjorie Freiman, Thomas Ulfelder, Colette Aufranc, Beth Sullivan Woods, Kenneth
5 Largess

6 **Also Present:** Executive Director Meghan Jop, Assistant Executive Director Corey Testa

7

8 **Meeting Documents:**

- 9 1. Agenda
- 10 2. Select Board Calendar
- 11 3. Meeting Overview
- 12 4. Memo from Town Clerk KC Kato Re: Election Staff Supplemental Appointments dated
13 12/31/25
- 14 5. Memo to Select Board from the Policy Subcommittee Re: Appointment Policy – Proposed
15 Revisions dated 12/2/25
- 16 6. Redlined Appointment Policy
- 17 7. Draft 2026 Town Meeting Article List
- 18 8. Draft 2026 Annual Town Meeting Warrant
- 19 9. Draft Minutes
 - 20 a. November 18, 2025
 - 21 b. November 25, 205
 - 22 c. December 2, 2025
 - 23 d. December 11, 2025
- 24 10. Other Correspondence
 - 25 a. Save the Date Reading Frederick Douglas Together
 - 26 b. Swellesley Report: Volunteers rally to throw holiday parties for Wellesley seniors
27 in public housing

29 1. **Call to Order**

30 Ms. Freiman called the Select Board meeting to order at 6:30 PM in the Juliani Room, Town Hall.
31 Ms. Freiman announced that the meeting was being broadcast live on Comcast Channel 8 and
32 Verizon Channel 40 and would be available for later viewing on wellesleymedia.org.

33 2. **Citizen Speak**

34 **Lisa Moore**, a Wellesley resident and environmental educator for the Natural Resource
35 Commission (speaking on her personal behalf), read a letter she had sent to Governor Healey.
36 She requested the 40 acres of MassBay Forest at 40 Oakland Street be removed from the surplus
37 land inventory list, noting the contradiction between selling this forest and the Governor's own
38 25-year biodiversity initiative launched in August. Ms. Moore emphasized the importance of
39 preserving mature forests rather than trying to rebuild destroyed ecosystems and pointed out that
40 Wellesley has already lost hundreds of trees to recent development.

41 **Philip Hahnfeldt**, Chestnut Street, questioned whether alternatives to selling the land had been
42 explored, such as acquiring the land through a conservation trust with sufficient public support.

43 He noted the contradiction in potentially destroying unique natural resources for the very
44 purpose of sharing them through affordable housing development.

45 **Christine Duvivier**, Pine Street, expressed surprise that Wellesley has more housing and less
46 parkland than comparable cities and towns despite receiving the National Tree City Award in 2024.
47 She supported the Select Board's challenge to the state on the MassBay Forest issue and identified
48 two "false trade-offs" being presented: (1) that the forest must be destroyed to fund a MassBay
49 building, and (2) that the forest must be destroyed to get affordable housing. She emphasized that
50 the forest is a Metro Boston treasure used by residents from many surrounding communities.

51 **Ellen Fine**, from Needham, spoke about her experiences hiking through the forest and expressed
52 concern about the climate impact of removing acres of forest. She also mentioned potential historic
53 and cultural significance to indigenous peoples and suggested development could occur on already
54 partially developed spaces rather than destroying natural forests.

55 **Caroline Wilson** a Wellesley resident spoke about her frequent use of the park as a dog walker,
56 noting how busy the parks are and therefore her shock at the land being designated as surplus.. Ms
57 Wilson described the park as a third space, free for the community to enter noting the community
58 maintained the trails. structured.

59 **Paul Derrickson**, Riverdale Road, emphasized the rarity of having such a reservation so close to
60 downtown in a suburban town, and stressed that once land like this is built upon, it cannot be
61 recovered.

62 3. Executive Director's Report

63 Executive Director Meghan Jop provided several updates:

- 64 • Ms. Jop reminded residents that the last date to pull nomination papers for Town-wide
65 office is Friday, January 9, at 5pm, with papers due by Tuesday, January 13, at 5pm with
66 50 signatures. For Town Meeting members, the last day to pull papers is January 23, with
67 a January 27 deadline and only 10 signatures required from the candidate's precinct.
- 68 • Ms. Jop noted it was annual renewal time for parking passes and dog licenses. Dog license
69 renewals must be completed by March 31 to avoid fines. Annual parking permits cost \$480
70 for residents, students, or business employees, and \$1,080 for non-residents. She
71 emphasized that annual passes are more economical for those using parking lots more than
72 13 times per month.
- 73 • Ms. Jop announced MassDOT overnight closures starting that evening for ramps from I-
74 95/Route 128 northbound and Mass Pike eastbound/westbound from 10pm-5am, with
75 detours in place.

76 4. Consent Agenda

78 Appoint Election Workers

79 Ms. Freiman reviewed the consent agenda items and confirmed with Ms. Jop that there had been
80 no requests for removal of any items from the Consent Agenda.

82 **Upon a motion by Ms. Aufranc and seconded by Mr. Ulfelder, the Board voted (5-0) to**
83 **approve the Consent Agenda.**

84

85 **Aufranc – Aye**

86 **Freiman – Aye**

87 **Largess – Aye**

88 **Sullivan Woods – Aye**

89 **Ulfelder – Aye**

90 **5. Policy Subcommittee Update**

91 **Discuss and Vote Appointment Policy Amendments**

92 Ms. Aufranc presented the draft Appointment Policy amendments that had been distributed to the
93 Board in December. She noted feedback had been received and circulated to the Board. The
94 Board had a detailed discussion about several aspects of the policy:

- 95 1. A section titled "Broad Community Engagement" was revised to "Consideration of Other
96 Town Service" to better reflect the content, and language suggesting that holding multiple
97 positions was inherently beneficial was removed.
- 98 2. In procedure step 2, the language was modified to clarify the consultation process
99 between the Select Board chair/liaison and appointed board chairs regarding needs and
100 qualifications.
- 101 3. Procedure step 3 was simplified focusing on notifying members whose terms are
102 expiring and asking if they will be applying for reappointment.
- 103 4. The term "Underlying Board" was changed throughout the document to "Appointed
104 Board" for consistency with other town documents.
- 105 5. Procedure step 6A was revised to clarify that the Select Board will determine whether
106 and how its members will participate in the interview process for appointments and
107 establish a process for individual board members to request additional information via
108 staff.

109 The Board agreed that the policy should make it clear that while the Select Board has ultimate
110 responsibility for appointments, the appointed boards will have appropriate input in the process.

111 **Upon a motion by Ms. Aufranc and seconded by Mr. Ulfelder the Board voted (5-0) to**
112 **approve the Appointments Policy, as amended, with final editorial and review by Town**
113 **Counsel to be integrated into the policy at the Policy Subcommittee meeting, and if**
114 **necessary bring back to the Select Board for further review.**

115 **Aufranc – Aye**

116 **Freiman – Aye**

117 **Largess – Aye**

118 **Sullivan Woods – Aye**

119 **Ulfelder – Aye**

120 **6. Annual Town Meeting Preparation**

121 **Review Draft Warrant**

122 Ms. Jop presented the draft warrant for Annual Town Meeting, noting it was currently being
123 reviewed by Town Counsel. With 37 articles, she described it as the shortest warrant in recent
124 memory, with several articles carried over from the previous year.

125 Ms. Jop noted the Board had already discussed several supplemental appropriations, the division
126 of the omnibus budget, and continued with an overview of other key articles.

127 The Board had an extended discussion about the DPW campus project feasibility study and how
128 it fits into larger capital planning efforts. Concern was expressed about the significant cost
129 implications and board members requested more information ahead of the joint meeting with the
130 Board of Public Works.

131 Ms. Jop explained that the feasibility study is part of a broader approach to town-wide capital
132 planning, with similar studies planned for the Fire Stations and in early discussions for School
133 buildings. This would allow the Town to develop a comprehensive 10–15-year plan with clear
134 financial implications for taxpayers. Preliminary estimates suggest major projects could increase
135 the median tax bill by approximately \$2,000 over a 4-year period.

136 Board members emphasized the importance of transparency about the cumulative impact of
137 multiple large capital projects on taxpayers, noting that operational costs also increase annually
138 and property valuations continue to rise. There was consensus that the Town needs a Town-wide
139 Facilities Master Plan to complement the Town-wide capital planning work.

140 The Board identified the need for additional joint meetings with various committees to discuss
141 articles and coordinate efforts including the Human Resources Board regarding when HR policies
142 have a financial implication, the Natural Resources Commission regarding the proposed financing
143 structure for the Land Conservation Plan and Wights Pond, and the Planning Board regarding
144 amendments to the Residential Incentive Overlay zoning bylaw amendments.

145 **7. Administrative Matters**

146 **Discuss and Vote Minutes**

147 The Board reviewed the minutes of November 18, 2025, November 25, 2025, December 2, 2025,
148 and December 11, 2025.

149
150 **Upon a motion by Ms. Aufranc and seconded by Mr. Ulfelder, the Board voted (5-0) to
151 approve the minutes of November 18, 2025, November 25, 2025, December 2, 2025, and
152 December 11, 2025, as amended.**

153
154 **Aufranc – Aye**

155 **Freiman – Aye**
156 **Largess – Aye**
157 **Sullivan Woods – Aye**
158 **Ulfelder – Aye**

159 **8. Chair's Report**

160 Ms. Freiman reported that she and the Vice Chair did not meet with the School Committee that
161 morning but did meet with the Advisory Committee. She noted that the chair of Advisory would
162 be presenting his analysis of longitudinal school expenditures the following evening, and that the
163 School Committee liaisons would be incorporating this work into their write-ups.

164 Ms. Jop added that the Town-wide Financial Plan presentation to Advisory has been moved to
165 February 25, which would allow for finalization of health insurance numbers.

166 **9. Executive Session**

167 At 7:45 pm, Ms. Freiman asked for a motion to enter into Executive Session to conduct strategy
168 for potential litigation with the Commonwealth regarding the disposition of surplus MassBay
169 Community College land as having the discussion in open session would be detrimental to the
170 Town's position.

171 **Upon a motion by Ms. Aufranc and seconded by Mr. Ulfelder, the Board voted by roll call
172 vote all aye (5-0) to enter executive session under Mass General Law chapter 30A sub section
173 21A exemption number 3 to conduct strategy for potential litigation with the Commonwealth
174 regarding the disposition of surplus MassBay Community College land and to invite Meghan
175 Jop, Corey Testa, Town Counsel Tom Harrington and Eric Reustle, and Special Counsel
176 Nick Shapiro and Robbie Hopkins to join as the Chair has declared that having such
177 discussions in open session would have a detrimental effect on the Town's position. Following
178 the adjournment of executive session, the Board will return to open session for the sole
179 purpose of adjourning the meeting.**

180 **Aufranc – Aye**
181 **Freiman – Aye**
182 **Largess – Aye**
183 **Sullivan Woods – Aye**
184 **Ulfelder – Aye**

185 **10. Adjournment**

186 The meeting was adjourned at 10:00 pm

187
188 The next regular meeting is scheduled for January 13, 2026, at 6:30 pm in Juliani Room, Town
189 Hall.

190

182

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184 February 25, which would allow for finalization of health insurance numbers.

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198 discussions in open session would have a detrimental effect on the Town's position. Following
199 the adjournment of executive session, the Board will return to open session for the sole
200 purpose of adjourning the meeting.**

201

202 **Aufranc – Aye**
203 **Freiman – Aye**
204 **Largess – Aye**
205 **Sullivan Woods – Aye**
206 **Ulfelder – Aye**

207

208 **10. Adjournment**

209 The meeting was adjourned at 10:00 pm
210
211 The next regular meeting is scheduled for January 13, 2026, at 6:30 pm in Juliani Room, Town
212 Hall.
213

11. 8:35 Chair's Report

Other Correspondence:

- ❖ Police Commendations
- ❖ Liaison Update – Largess
- ❖ Library Trustees Comments on TWCPC



TOWN OF WELLESLEY

WELLESLEY, MA 02482

Telephone 781-235-0062



POLICE DEPARTMENT

Scott Whittemore

Chief of Police

TO: OFFICER MIKE PINO
OFFICER ERIC FERRARA

FROM: CHIEF SCOTT WHITTEMORE

SUBJECT: LETTER OF COMMENDATION

DATE: JANUARY 20, 2026

I was pleased to receive the attached email from Wellesley resident David Himmelberger regarding a recent encounter while conducting traffic enforcement on Linden Street.

To summarize the event, Officer Ferrara stopped a vehicle for a traffic/motor vehicle infraction. During that stop, it was discovered that the driver had a suspended Massachusetts driver's license. Officer Pino arrived on the scene to assist. Mr. Himmelberger was not involved in the incident, but watched the episode unfold from his front yard.

Mr. Himmelberger was impressed with the professionalism and compassion that both of you displayed to the driver. To quote Mr. Himmelberger directly, “..it is so refreshing to see good police work that fairly addresses the situation at hand, and is respectful of the stopped individual.”

I appreciate the continued hard work, dedication, and empathy that both of you demonstrate in service to our community. Clearly, the residents of Wellesley notice and appreciate your efforts.

AUTHORIZED:

SCOTT WHITTEMORE
CHIEF OF POLICE



TOWN OF WELLESLEY

WELLESLEY, MA 02482

Telephone 781-235-0062



POLICE DEPARTMENT

Scott Whittemore

Chief of Police

TO: OFFICER MIKE PINO
OFFICER EVAN ROSENBERG

FROM: CHIEF SCOTT WHITTEMORE

SUBJECT: LETTER OF COMMENDATION

DATE: JANUARY 20, 2026

I recently received an email from a lifelong Wellesley resident who wanted to thank you both for your service. Both of you responded to the resident's dwelling on a 'well-being' check after a friend was unable to get in touch with the resident and became concerned.

As noted in the attached email, the resident was feeling a bit under the weather and had overslept. She described you both as courteous, concerned, and very friendly, despite being woken up by the police. She also wanted to ensure that you knew she was appreciative of your efforts.

Your work in this matter is indicative of the type of community service that the Wellesley Police Department performs on a daily basis.

AUTHORIZED:

A handwritten signature in black ink, appearing to read "Scott Whittemore".

SCOTT WHITTEMORE
CHIEF OF POLICE

K. Largess Liaison Updates – January 18, 2026

- RIO Task Force
 - The Task Force is working to schedule another meeting in late January/early February, the purpose of which is unclear.
 - The Planning Board, per the recommendation of the Task Force, would like revisions to RIO to include a requirement that a developer enter into a Development Agreement with the Town if the developer intends to use the RIO bylaw.
 - Marjorie asked Planning to respond to various questions about the Development Agreement requirement, including specific language for the SB to consider. Planning intends to present this to the SB in the coming weeks.
- Library Trustees
 - See comments from the Trustees on the Capital Planning Committee.
- Affordable Housing Trust
 - The Trust is working on hiring a consultant to help us formulate a 5 year plan.
- Wellesley Housing Authority
 - The WHA recently hired the Cambridge Housing Authority to provide certain management and administrative services to the WHA (e.g., administrative oversight, property management, compliance & reporting, etc.).

The Library Trustees wish to acknowledge and thank the Select Board and its Policy Subcommittee for their extensive work in drafting elements of the proposed Capital Planning Committee. We are grateful for the opportunity to provide feedback on the plan as currently proposed. Understanding that the Select Board continues to refine the draft, we look forward to continuing updates.

It goes without saying that long term planning is an essential part of good governance. With the ever-growing needs – some anticipated and others coming up organically – and the limited resources available to meet these growing costs, setting priorities and timelines must be done. While most, if not all, town departments have done their own planning which has been reflected in the town's 5-year capital plan, this work has not been shared with the public along the way. As proposed, this new committee will do its work as a public body, complete with open meetings, agendas and minutes and provide the community an opportunity to follow and make comment as they see fit. The committee will extend the planning horizon out to ten years which is important for longer term planning. These are very positive aspects of the proposed concept. However, within a ten-year horizon, priorities and needs can change so Trustees recommend that the committee does an annual review of the project timeline and revise as necessary.

Our primary concern with the draft we have reviewed involves the make-up of the proposed committee. We appreciate the rationale of choosing to represent only those departments with "capital intensive" needs but disagree with the premise. The Trustees feel strongly that all elected boards and town departments who have ownership or custody of town property should be full members of the proposed group. As currently proposed, it seems that there are only two departments who are excluded – the Library and the NRC. We do not presume to speak for NRC.

The Trustees have care and custody of three town buildings and for purposes of planning and townwide integration of timelines, resource sharing and overall coordination, we feel it is important to be part of the group making these decisions. It has been suggested that we need a townwide facilities master plan which would surely require input from all departments that have responsibility for buildings. While the library does not have as many projects that meet the threshold for discussion as others on the committee, the library may indeed have projects that will need to be included on the timeline in the future. Without full membership on the committee, the proposal to simply come before the group to petition for a project and have no vote on the outcome seems a poor substitute. Departments who have responsibility for buildings, should be present as committee members with voting rights.

The Select Board has made it a point to stress the goal of individual town departments collaborating effectively to function as “one town” and have worked to bring that concept into reality. Regretfully, excluding only two departments/boards from full participation appears to undermine that effort. Again, NRC may or may not wish to participate so it could end up adding only one more seat.

Full participation by all property stakeholders encourages collaboration among town staff and departments, allows the committee to benefit from departmental expertise and experience, and builds stronger support for the timeline and plans that are developed. It is collaborative rather than exclusionary, democratizes the process and promotes the goal of a unified and aligned town. Everyone benefits.

The Trustees look forward to continued dialogue as the draft for the committee evolves. We support long term capital planning and the Select Board’s effort to move this concept forward. In that vein, we respectfully ask that the Trustees and the WFL department have a firm role in that effort as full members of the proposed committee.